



Oklahoma Historical Society
State Historic Preservation Office

Founded May 27, 1893

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

September 23, 2022

Mr. Scott Sundermeyer
ODOT Cultural Resources Program Manager
3200 Marshall Avenue, Room 110
Norman, OK 73072

RE: File #1784-22 [Previously #3071-21]; Bird Creek Bridge on SH-66 Improvements Project,
Revised Design, Project #JP-20899(09), Rogers County

Dear Mr. Sundermeyer:

We have received and reviewed the additional documentation submitted with your letter dated August 29, 2022, for the referenced project. The project, as proposed, will have an **adverse effect** on the Bird Creek Bridge (a.k.a. Verdigris River Bridge) #6602-0368-EX/NBI-13688, a property previously determined eligible for listing in the National Register of Historic Places (Consensus DOE, C, 11/21/2007).

In response to your discussion about the intent and purview of Stipulation III.B of the existing, amended Memorandum of Agreement (MOA #334): *Regarding Adverse Effect to State Highway 66 Bridge Over Bird Creek* (executed 10/07/2021), we find that ODOT's efforts to maintain the functional life of the 1956 bridge were not adequate. The document ODOT provided for our reference, FHWA's *Bridge Preservation Guide*, outlines examples of cyclical and condition-based maintenance activities that can be implemented in order to keep a historic bridge functional for today's and future use, of which guidance from the document was used for maintenance projects on the bridge. However, it is the opinion of the OKSHPO that the extent of the projects that were undertaken to maintain the remaining 1956 bridge were minimal in scope and did not achieve the goal of Stipulation III.B. In *Table 3 Examples of condition-based maintenance activities* on Page 6 of the *Guide*, there are clearly additional maintenance options that could have been undertaken to make more substantial repairs to the bridge, including, but not limited to, steel member repair and fatigue crack mitigation, that would have increased its usable life. The fact that the only projects that were undertaken on the bridge, as noted in the list provided with your cover letter, were limited to the deck and joints, tells us that little to no considerations and repairs were done to the superstructure or substructure. This can be considered "demolition by neglect" and is not in keeping with intent and spirit of Stipulation III.B of the existing, amended MOA #334.

Thank you for providing a copy of the Advisory Council on Historic Preservation (ACHP) notification declining participation at this time. We look forward to consulting with the Federal Highway Administration (FHWA) on a new MOA to resolve or mitigate the adverse effect to this historic property. Upon the execution of the MOA, a copy must be filed with the ACHP to complete the Section 106 process.

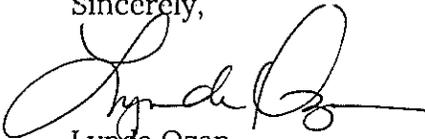
Thank you for the opportunity to review this project. If you have any questions, please do not hesitate to call Kristina Wyckoff, Historical Archaeologist, at 405-521-6381 or Sara Werneke, Historic Preservation Architect, at 405-522-4478.

Mr. Sundermeyer
September 23, 2022
Page 2

RE: File #1784-22 [Previously #3071-21]; Bird Creek Bridge on SH-66 Improvements Project,
Revised Design, Project #JP-20899(09), Rogers County

Please reference the above underlined file number when responding. Thank you.

Sincerely,

A handwritten signature in black ink, appearing to read 'Lynda Ozan', with a long horizontal flourish extending to the right.

Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm



September 6, 2022

Karen Orton
Realty Officer/Environmental Program Manager
Federal Highway Administration – Oklahoma Division
5801 N. Robinson Avenue, Suite 300
Oklahoma City, OK 73118

Re: Documentation of Consultation under Section 106 of the NHPA for the Rogers County Federal-aid project STP-266C(116)PM, J/P 20899(09) State Highway 66 over Bird Creek.

Dear Ms. Orton:

Introduction

The Oklahoma Department of Transportation (ODOT) proposes a federal-aid highway funded bridge replacement project near Catoosa, Oklahoma. The project includes replacement of the structure carrying SH-66 over Bird Creek (Bird Creek Bridge). The subject bridge is eligible for listing on the National Register of Historic Places (NRHP) under criterion C.

The proposed undertaking includes replacement of the existing structure on existing alignment by constructing a new 38-foot-wide bridge with approach roadway with two 12-foot driving lanes, 10-foot-wide paved outside shoulder, and four-foot inside shoulder similar to the structure constructed on the southbound lanes in 2012. The new structure would be constructed within existing right-of-way, which would eliminate impacts to a nearby publicly-owned park, Rogers Point Park.

In consultation with the Oklahoma State Historic Preservation Officer (SHPO), the Oklahoma Department of Transportation (ODOT) reviewed National Register Bulletin 15 and applied the Criteria of Adverse Effect found in 36 CFR 800.5 to this undertaking and has determined that there will be an adverse effect to the National Register-eligible bridge through the replacement of this structure. SHPO concurred that the selection of this alternative would constitute an adverse effect.

Project History

On behalf of FHWA, ODOT initiated consultation with SHPO and consulting parties in 2019. In May, 2020, ODOT prepared and submitted a cultural resources report of investigations to SHPO and consulting parties. SHPO concurred that, with the exception of the National Register-eligible bridge, there were no other historic properties within the area of potential effect (APE).

ODOT has held one Section 106 consulting party meeting on September 20, 2021 and one Virtual Open House that was available from June 12 through June 27, 2022. ODOT continually maintained and updated the Section 106 consultation materials and status updates a project website available for the public (<http://www.odotculturalresources.info/bird-creek-bridge.html>). The public meeting materials are available also available online(<https://oklahoma.gov/odot/programs-and-projects/public-meetings-and-hearings/20220613.html>).

Project Submittal

We are submitting project documentation to your office and requesting FHWA invite Council participation in accordance with 36 CFR 800.6(a)(1).

In accordance with 36 CFR 800, ODOT has completed the following.

800.4(a)(1) - The APE is defined as the existing right-of-way needed for the project. All work will be completed within existing right-of-way.

800.4(a)(2) - Review of existing information consisted of researching the National Register of Historic Places in Oklahoma, the Oklahoma Determination of Eligibility database, and the archaeological site files at the Oklahoma Archaeological Survey. As indicated in the attached consultation, there are no other historic properties within the APE.

800.4(a)(3) – ODOT held a consulting party meeting for the referenced undertaking. Consulting parties included SHPO, the State Archaeologist, the City of Catoosa, National Park Service (Kaisa Barthuli and Meg Frisbee), Historic Bridge Foundation, Oklahoma Route 66 Association, Route 66 Road Ahead Partnership, Preservation Oklahoma, Nathan Holth, and Oklahoma Tourism. With the exception of comments from SHPO and Native American Tribes, no comments have been received from Section 106 consulting parties throughout the duration of the consultation period.

In addition to the above, ODOT held a virtual open house from June 12 through June 27, 2022. A total of 515 unique users viewed the website over the two-week period. Twenty-three members of the public signed in on the website; however, sign in was not required. A total of 44 public comments were received; the majority expressed a preference for a specific alternative, noted the historical significance of the bridge and/or provided recommendations for mitigation. There were no additional comments that pertained to a common concern or consideration.

800.4(a)(4) – Consultation with Native American tribes was conducted via letters containing project information and scope, in accordance with existing agreements and protocols FHWA have with the tribes. For this undertaking, ODOT sent letters to nine federally recognized Tribes in Oklahoma who have demonstrated their interest in being consulted in Rogers for FHWA projects (Caddo Nation, Cherokee Nation, Delaware Tribe of Indians, Kialegee Tribal Town, Muscogee (Creek) Nation, Osage Nation, Thlophlocco Tribal Town, United Keetoowah Band of Cherokee, and Wichita and Affiliated Tribes). The final report of investigations was also submitted to these tribes. The following comments were received:

- Cherokee Nation: October 31, 2019 and May 29, 2020 responded that there were two culturally sensitive areas in close proximity, but outside the APE for the undertaking and requested avoidance of those areas.
- Delaware Tribe of Indians: October 9, 2019 and May 5, 2020 responded that there are no religious or culturally sensitive sites in the project area.
- Muscogee (Creek) Nation: October 3, 2019 responded that the project was located outside of their area of interest.
- Osage Nation: January 11, 2020 responded that there are known known Osage resources within the project area.

In addition to the above, tribes consulted as part of this undertaking will be invited to sign the MOA as concurring parties.

800.4(c) – ODOT's conducted cultural resources surveys within the APE to determine if properties met National Register criteria. With the exception of the historic bridge, no other historic properties were identified.

800.5 – ODOT applied the Criteria of Adverse effect and determined that the proposed bridge replacement would adversely affect the Bird Creek Bridge. SHPO provided comment on June 21, 2022 concurring with the effect finding. In addition, the SHPO provided additional comment regarding the maintenance activities that ODOT had implemented as part of the Memorandum of Agreement for a previous project. ODOT responded to that comment citing additional maintenance presented in our consulting party meeting.

At this time, ODOT and SHPO have proposed to address adverse effects in a Memorandum of Agreement. All consulting parties will be invited to sign on as concurring parties.

The following documents are enclosed for your review and consideration:

- The Cultural Resources Report
- Consultation to date with the Oklahoma SHPO and Oklahoma State Archaeologist and consulting parties
 - ODOT Cultural Resources Program (CRP) maintained a project website that contains consulting party meeting videos and PowerPoint presentations, as well as additional information that was provided to consulting parties throughout the process (<http://www.odotculturalresources.info/bird-creek-bridge.html>)
- All consultation with Native American Tribes
- Slide presentation from consulting party meeting
- The Design Support for Section 4(f) Analysis for Historic Bridges

The above-referenced documentation regarding the referenced project should be submitted to the Advisory Council on Historic Preservation, pursuant to Section 800.6(a)(1) of the Section 106 regulations. Please inform our office when this information has been forwarded to the ACHP so that ODOT-CRP may provide this documentation to other consulting parties, as stipulated in 36 CFR 800.6(a)(3).

Once we receive Council comment, ODOT will prepare a Memorandum of Agreement (MOA) for FHWA execution. FHWA, SHPO and the Council (if participating) will be signatories to the MOA. ODOT will be invited signatories. ODOT anticipates that all current Section 106 consulting parties will be concurring parties to the MOA. Native American tribes will be invited to participate as concurring parties.

If you have any questions regarding this project, please contact me at 405-325-7201.

Sincerely,



Scott A. Sundermeyer, RPA
ODOT Cultural Resources Program Manager

attachments

August 29, 2022

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Re: File 1784-22; Rogers County Federal Highway Administration (FHWA) Project: JP 20899(09);
Improvements to SH-66 bridge over Bird Creek

Thank you for your comments of June 21, 2022 and for your continued participation in the proposed undertaking. Your recent comment was in regards ODOT and FHWA's commitment to adhering to Stipulation III(B) of the Memorandum of Agreement for JP 20899(04) (MOA #334).

B. ODOT will continue to implement feasible and reasonable repairs as necessary to maintain structural integrity and load carrying capacity of the bridge until it is no longer feasible and prudent.

i. ODOT and FHWA shall consult with Oklahoma SHPO and consulting parties for those repair and maintenance projects that are defined as federal undertakings, in accordance with 36 CFR 800.16(y).

We appreciate your recognition of the two undertakings for a bridge paint and a joint seal repair that were submitted for review and comment (JPs 20926(04) and 20926(05), respectively), however we disagree that these two projects do not demonstrate a commitment to the fulfillment of Stipulation III(B):

FHWA's Bridge Preservation Guide includes several sections that specifically discuss bridge maintenance and preservation activities intended to maintain structural integrity. Bridge preservation is defined as actions or strategies that prevent, delay or reduce deterioration of bridges or bridge elements; restore the function of existing bridges; keep bridges in good or fair condition; and extend their service life. Preservation actions may be cyclic or condition-driven. As indicated in the document, effective bridge preservation actions are intended to delay the need for costly rehabilitation or replacement while bridges are still in fair or good condition.

Condition-based maintenance activities include Joint Seal Replacement, Joint Repair/Replace/Elimination, and Spot/Zone/Full Painting Steel Elements, among other activities. As indicated in the bridge inspection report table below, the 20926(05) undertaking met these conditions and raised the structural rating from a "poor" to "fair".

ODOT also consulted with your office for a bridge paint project under JP 20926(04) and received comment concurring with our *no adverse effect* finding, based on the fact that the undertaking includes "Application of a new paint finish system to prevent further deterioration of bridge surfaces."

In addition to these two federal-aid undertakings, ODOT also conducted various other cyclical state-funded maintenance measures, which were not submitted to your office for review. However, these measures were presented in our September 20, 2021 consulting party meeting. A video of the meeting, meeting minutes, and the slide illustrating these maintenance activities has also been posted on the project website since October 2021. Those cyclical maintenance activities included:

"The mission of the Oklahoma Department of Transportation is to provide a safe, economical, and effective transportation network for the people, commerce and communities of Oklahoma."

Repair Bridge Deck – April 2019
 Clean/Rehabilitate/Replace Bridge Joints - September 2019
 Repair Bridge Deck – January 2020
 Rehabilitate Floor System – March 2020
 Repair Bridge Deck – June 2020
 Repair Bridge Deck - November 2020
 Continuous maintenance (deck washing)

ODOT has been diligent in maintaining the project website. This includes providing the bridge inspection reports from 2013 to 2020 for consulting party review. Please note the following bridge ratings as indicated on the two-year inspection cycle.

Date	Deck Rating	Superstructure Rating	Substruction Rating
11-21-2013	5- Fair	4- Poor	6- Satisfactory
11-16-2015	5- Fair	5- Fair	6- Satisfactory
11-16-2016	5- Fair	5- Fair	6- Satisfactory
11-9-2017	5- Fair	4- Poor	6- Satisfactory
11-6-2018	5- Fair	4- Poor	6- Satisfactory
11-14-2019	5- Fair	4- Poor	6- Satisfactory
11-20-2020	5- Fair	4- Poor	6- Satisfactory

Stipulation III(B) of MOA #334 states that ODOT will implement repairs until it is no longer feasible and prudent. A primary tenet of the Section 4(f) evaluation process is to review the feasibility and prudence of alternatives that address deficiencies in a historic bridge. Many of the alternatives that must be reviewed under a Programmatic Section 4(f) for historic bridges include actions that involve some level of rehabilitation in order to allow the structure to continue to carry vehicular or pedestrian traffic. Rehabilitation involves major work that is intended to “restore” the structural integrity of a bridge and/or incorporate work that may be necessary to correct major safety defects. ODOT completed a full review of rehabilitation alternatives and presented these in the Design Analysis document (submitted to your office on May 31, 2022). Rehabilitation alternatives were presented under Alternatives 2 and 3.

ODOT respectfully submits that the federal and state-funded maintenance measures were completed in good faith in accordance with the commitments outlined in Stipulation III(B). ODOT has selected Alternative 4 because it is no longer feasible and prudent to continue to maintain the structure at a condition rating of fair or better. We had hoped that the current condition of the bridge was clearly reflected in the Design Analysis report but are pleased to have been able to provide you with this additional information.

Please visit the webpage (<http://www.odotculturalresources.info/bird-creek-bridge.html>) to view all project documentation. If you have any questions or require any additional information, please do not hesitate to contact me at 405-325-7201 or ssundermeyer@odot.org.

Sincerely,

Scott Sundermeyer
 ODOT Cultural Resources Program Manager



Oklahoma Historical Society

Founded May 27, 1893

State Historic Preservation Office

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

June 21, 2022

Mr. Scott Sundermeyer
ODOI Cultural Resources Program Manager
3200 Marshall Avenue, Room 110
Norman, OK 73072

RE: File #1784-22 [Previously #3071-21]; Bird Creek Bridge on SH-66 Improvements Project,
Revised Design, Project #JP-20899(09), Rogers County

Dear Mr. Sundermeyer:

We have received and reviewed the documentation submitted on the referenced project. We concur with your finding that Alternative #4 will have an **adverse effect** on the Bird Creek Bridge (a.k.a. Verdigris River Bridge) #6602-0368-EX/NBI-13688 (Consensus DOE, C, 11/21/2007), a property previously determined eligible for listing in the National Register of Historic Places.

Please note that Alternative #4, demolition of the 1956 bridge and construction of a new bridge, is not in keeping with Stipulation III.B of the existing, amended Memorandum of Agreement, *MOA #334: Regarding Adverse Effect to State Highway 66 Bridge Over Bird Creek* (executed 10/07/2021). This stipulation was included in the original MOA, executed in 2011.

In the eleven years since the execution of the 2011 MOA, ODOT has consulted with the OK/SHPO on two projects that comprised maintenance or repairs to this bridge structure. These projects included bridge painting (File #1260-13, J/P-20926[04]) and joint and seal repairs (File #0852-15, J/P-20926[05]). Painting and joint and seal repairs do not “maintain the structural integrity or load carrying capacity of the bridge,” and as such, it is our opinion these projects do not demonstrate a fulfillment of the Stipulation III.B commitment.

We welcome the opportunity to continue consultation with you to mitigate, minimize, or eliminate the adverse effect of the proposed project. However, if we are unable to eliminate the adverse effect of the project, the Federal Highway Administration (FHWA) will need to contact and invite the participation of the Advisory Council on Historic Preservation (ACHP) in order to complete the Section 106 process as outlined in 36 CFR Part 800. Should the ACHP choose not to participate in the consultation, FHWA and the SHPO may execute an MOA. Upon the execution of the MOA, a copy must be filed with the ACHP to complete the Section 106 process.

Thank you for the opportunity to review this project. If you have any questions, please do not hesitate to call Kristina Wyckoff, Historical Archaeologist, at 405/521-6381. Please reference the above underlined file number when responding.

Sincerely,

Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm

ODOT CULTURAL RESOURCES PROGRAM

Search



Home

Documents and Toolkits

Cultural Resources Reports
that the Department has
produced

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Tribal Consultation

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IMPROVEMENTS TO SH-66 BRIDGE OVER BIRD CREEK, NEAR CATOOSA, OKLAHOMA

PROJECT BACKGROUND



Bridgeport Bridge

Bird Creek Bridge

Provide Input

SECTION 106 Public Involvement



Members of the public can be involved in the Section 106 process by identifying and evaluating resources that are historically important. You, as a member of the public, can participate in the process by expressing your concerns to the Oklahoma Department of Transportation (ODOT). It is important for you to let us know what is important to you and why it is important. Input can be provided at public meetings, by contacting representatives of ODOT, or by submitting public comments at the meeting or online at <http://www.odotculturalresources.info>.

The Section 106 review process gives you the opportunity to alert the federal agency to the historic properties you value, and influence decisions about projects that affect them. If you have a special interest in a property and would like to play a more formal role in the Section 106 process, you may want to become a consulting party.



To request consulting party status send a letter explaining your interest in the project or historic property to:

ODOT Cultural Resources
Oklahoma Archeological Survey
111 E. Chesapeake
Norman, OK 73019-5111

Consulting parties can include:

- Federal agencies
- State Historic Preservation Office
- Representatives of local governments with jurisdiction over the area with historic properties
- Applicants for federal funds
- Additional consulting parties include those with a *demonstrated interest* in the undertaking due to their legal or economic interest in the project or property OR the project effects on historic properties.

Consultation is the process of seeking, discussing and considering the views of others, and where feasible, seeking agreement with them on how historic properties should be identified, considered and managed. Consultation is built on the exchange of ideas.

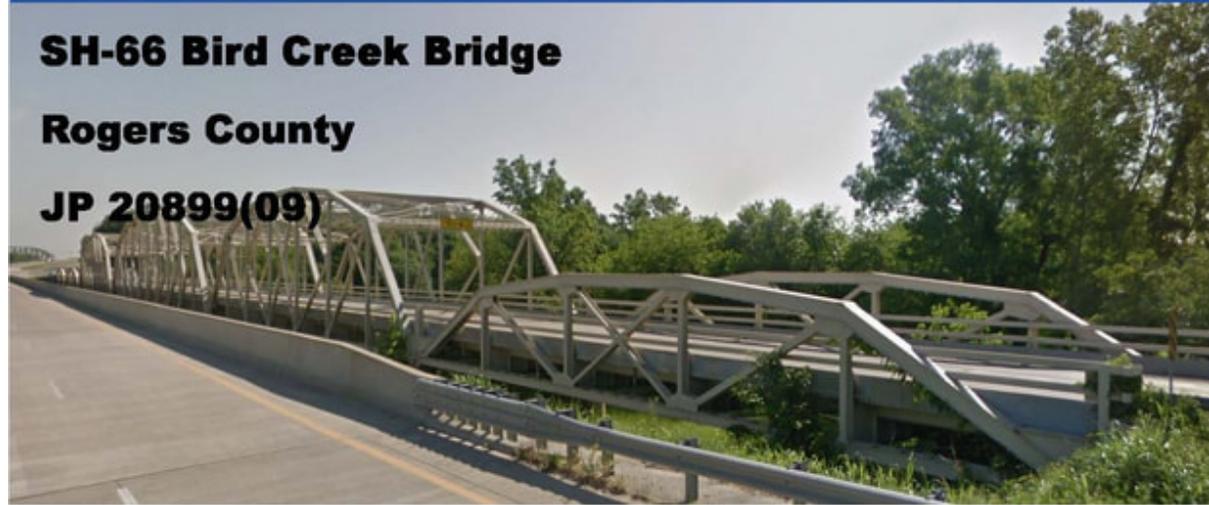
Consultation occurs at key stages of each project including:

- The identification of properties eligible for listing on the National Register of Historic Places,
- The assessment of effects on those properties, and
- Resolution of adverse effects.

SH-66 Bird Creek Bridge

Rogers County

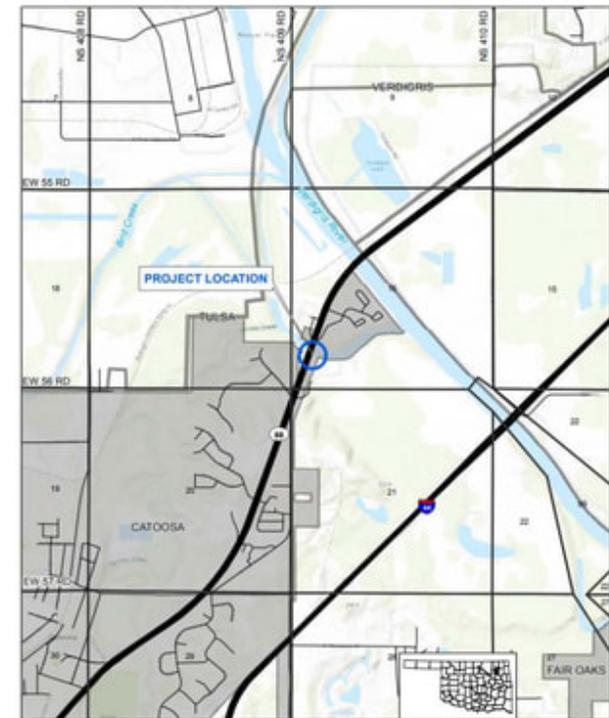
JP 20899(09)



Project Background

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), is considering improvements to the historic eastbound SH-66 bridge over Bird Creek. ODOT is currently undergoing a study to evaluate the structural integrity of the bridge, and review a variety of alternatives that may address the deficiencies in the structure.

The bridge is mixed truss, containing three K-thru truss spans that are flanked by two pony trusses on the east and one pony truss on the west. The most recent inspection report lists the sufficiency rating at 49.40 out of 100. According to data obtained in 2016, the structure carries 6,750 vehicles per day. The structure, along with its former twin, which was replaced in 2012, was identified by the 2002 Route 66 Roadbed Study as best representing the upgrade of Route 66 to a modern 4-lane highway.



Should you have any questions regarding the project, please contact Mr. Scott Sundermeyer, ODOT Cultural Resources Program Director, at (405) 325-7201, ssundermeyer@odot.org.

Maintenance and other repairs

- 20926(04) – Bridge Paint project – consulted Spring 2013
- 20926(05) – Bridge Joint/Seal Repair – consulted February 2015

- Repair Bridge Deck – April 2019
- Clean/Rehabilitate/Replace Bridge Joints - September 2019
- Repair Bridge Deck – January 2020
- Rehabilitate Floor System – March 2020
- Repair Bridge Deck – June 2020
- Repair Bridge Deck - November 2020



- **October 1, 2021 (posted).** [Meeting Minutes from September 20, 2021 meeting.](#)
- **September 20, 2021 Section 106 consulting party meeting.** This link takes you to the video recording of the consulting party meeting.

- **Presentation.** This link opens the consulting party meeting presentation.
- **Design Analysis (May 2022).** This document represents the engineering report and includes an existing conditions analysis and discussion of the alternatives that must be considered under Section 4(f).
 - ****This document was initially completed in September 2021, but was updated in May 2022 to reflect a March 2022 bridge inspection.**
- **Section 106 Initiation letter to consulting parties - November 11, 2019**
- **Cultural Resources Report provided to consulting parties May 12, 2020**
 - **Transmittal letter with report**
- **Bridge Inspection Reports (2013 through 2020); zipped file**

SUBMIT YOUR COMMENTS REGARDING THE SH-66 BIRD CREEK BRIDGE PROJECT HERE.

** Indicates required field*

Name *

First

Last

Email *

Comment *

Submit

From: [Scott Sundermeyer](mailto:Scott.Sundermeyer@odot.org)
To: kitty@historicbridgefoundation.com; rt66theroadahead@gmail.com; [Margaret Frisbie@nps.gov](mailto:Margaret.Frisbie@nps.gov); anneehaaker@gmail.com; jblish@cityofcatoosa.org; rhys.martin@oklahomaroute66.com; director@preservationok.org; kaisa_barthuli@nps.gov; [Lynda Ozan](mailto:Lynda.Ozan@odot.org); [Kristina Wyckoff](mailto:Kristina.Wyckoff@odot.org); [Nathan Holth](mailto:Nathan.Holth@odot.org); carrie.rush@travelok.com
Cc: [Anna Eddings](mailto:Anna.Eddings@odot.org); [Erin Faulkner](mailto:Erin.Faulkner@odot.org); [Greg Maggard](mailto:Greg.Maggard@odot.org); [Kelly Saladis](mailto:Kelly.Saladis@odot.org); [Jenny Droscher](mailto:Jenny.Droscher@odot.org)
Subject: Rogers County 20899(09) SH-66 Bird Creek Bridge - submittal of revised Design Analysis document
Date: Thursday, June 9, 2022 12:06:00 PM
Attachments: [SH66 virtual meeting handout.pdf](#)
[20220527_SH-66_stakeholder_letter.pdf](#)

Good afternoon all –

Since our last correspondence, ODOT has completed additional bridge inspections in November 2021 and March 2022 to produce a revised Design Analysis. This document was completed in May 2022. This revised analysis also discusses the maintenance projects completed in 2014 and 2015, and it includes copies of the November 2021 and March 2022 bridge inspections. I have posted the document at the project web page: <http://www.odotculturalresources.info/bird-creek-bridge.html>

After careful review of the alternatives that meet the purpose and need of the project, and the comments we have received to-date, ODOT believes Alternative 4 to be the best solution to address the crossing of SH-66 at Bird Creek.

- Alternative 4 includes constructing a new bridge on the existing alignment. The proposed structure would be concrete slab on beam structure similar to the southbound bridge constructed as part of the previous undertaking.
- This alternative meets the purpose and need of the project by providing a long-term solution to the transportation challenge at this crossing and will provide a crossing that provides all drivers a greater comfort level on the facility. If approved by FHWA, this alternative would constitute a Section 4(f) use and an adverse effect to the historic bridge.

ODOT is submitting the Design Support to Section 106 consulting parties for review, understanding the purpose and need of the project, existing conditions of the structure, impacts to other environmental resources, and the long-term solution that Alternative 4 presents.

As we continue the project planning, public involvement, and alternatives review, ODOT will host a Virtual Open House from June 13 through June 27, 2022. All Section 106 consulting parties, Native American Tribes, and the public will receive a mailed invitation on ODOT letterhead to the open house. I have also attached those materials to this correspondence. The meeting and other public involvement materials will be placed on the main project website at www.odot.org/SH66NB.

Once the Open House has concluded, ODOT will review all public comments and incorporate the results of our Section 106 consultation and other environmental reviews and make a determination as to which alternative to submit to FHWA. ODOT must carefully consider several factors in its decision to pursue FHWA approval of the project. Many of these factors include constraints that other state and federal agencies consider important. ODOT will also weigh this alternative with comments from the public.

From previous correspondences, email transmittal of this information has been preferred by our Section 106 consulting parties. If you would like this information submitted in hard copy, please do not hesitate to reach out to me. Please do not hesitate to reach out with any other comments or questions as well. I very much encourage your participation in the Virtual Open House.

Best-
Scott

May 31, 2022

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Re: File 3071-21; Rogers County Federal Highway Administration (FHWA) Project: JP 20899(09); Improvements to SH-66 bridge over Bird Creek; Revised Design Support report submittal, selection of proposed alternative, and determination of effect submittal for comment under the National Historic Preservation Act (NHPA).

Introduction

Thank you for your comments of October 19, 2021 and for your continued participation in the proposed undertaking. In accordance with your comments regarding errors on page 3 of the Design Analysis, we have addressed those corrections. Since our last correspondence, ODOT has completed additional bridge inspections in November 2021 and March 2022. ODOT has combined the results of these inspections and has made additional terminology corrections in the previous document to produce the attached revised Design Analysis, which was completed in May 2022. This revised analysis also discusses the maintenance projects completed in 2014 and 2015.

The attached revised Design Analysis report documents existing conditions of the bridge and assesses the alternatives that must be reviewed for a Programmatic Section 4(f) for historic bridges. In accordance with 23 CFR 774, FHWA may not approve of the “use” of a Section 4(f) property unless there is no feasible and prudent alternative, and the action includes all possible planning to minimize harm.

The bridge consists of 30-foot-wide clear roadway, with an approach roadway of 37’-1”. The current average daily traffic is 7900 vehicles per day. Truck traffic accounts for 7 percent of the traffic. While the bridge is not currently load posted, the structure has deteriorated significantly since a November 2021 inspection, specifically the floor beams and stringers which has resulted in a superstructure rating of 4 (poor condition).

With the exception of comments from SHPO, ODOT has received no comments from other consulting parties as a result of our September 20, 2021 consulting party meeting or solicitations. With the completion of the attached revised Design Analysis, ODOT will host a Virtual Open House from June 13 through June 27, 2022. All Section 106 consulting parties, Native American Tribes, and your office, will receive a mailed invitation on ODOT letterhead to the open house. The meeting and other public involvement materials will be placed on the main project website at www.odot.org/SH66NB.

Discussion of Alternatives; ODOT Submittal for FHWA Approval

After careful review of the alternatives that meet the purpose and need of the project, and the comments that SHPO provided on October 19, 2021, ODOT plans to submit Alternative 4 to FHWA for approval.

Alternative 4 includes constructing a new bridge on the existing alignment. The proposed structure would be concrete slab on beam structure similar to the southbound bridge constructed as part of the 20899(04) undertaking.

“The mission of the Oklahoma Department of Transportation is to provide a safe, economical, and effective transportation network for the people, commerce and communities of Oklahoma.”

This alternative meets the purpose and need of the project by providing a long-term solution to the transportation challenge at this crossing and will provide a crossing that provides all drivers a greater comfort level on the facility. If approved by FHWA, this alternative would constitute a Section 4(f) use and an adverse effect to the historic bridge.

Summary

In accordance with 36 CFR 800.5(a), ODOT has applied the criteria of adverse effect and determined that Alternative 4 will have an adverse effect to the historic Bird Creek Bridge, a property eligible for listing on the NRHP.

As SHPO is the Official with Jurisdiction over Section 4(f) historic properties, ODOT is submitting the Design Support to your office for review and comment. We respectfully request comment on ODOT's decision to pursue the approval of Alternative 4 from FHWA and the prudence of that alternative when considering the purpose and need of the project, existing conditions of the structure, impacts to other environmental resources, the long-term solution that Alternative 4 presents, and the cost of the alternative.

We also request comment on ODOT's opinion of effect and any measures that should be considered in order to avoid or minimize adverse effects to the historic property. In addition, ODOT invites SHPO and other consulting parties to attend the Virtual Open House and provide comment on the proposed alternative.

Once the Open House has concluded, ODOT will incorporate the results of our Section 106 consultation, and other environmental reviews, and submit the 4(f) analysis and Alternative 4 to FHWA for approval. At that time, ODOT will also request that FHWA submit documentation to the Advisory Council on Historic Preservation and invite their participation, in accordance with 36 CFR 800.6(a)(1).

ODOT must carefully consider several factors in its decision to pursue FHWA approval of Alternative 4. Many of these factors include constraints that other state and federal agencies consider important. ODOT will also weigh this alternative with comments from the public.

Please visit the webpage (<http://www.odotculturalresources.info/bird-creek-bridge.html>) for all project documentation. If you have any questions, Please do not hesitate to contact me at 405-325-7201 or ssundermeyer@odot.org.

Sincerely,



Scott Sundermeyer
ODOT Cultural Resources Program Manager

Attachments

Design Support for Section 4(f) Analysis for Historic Bridges

Structure No. 6602 0368 EX (NBI No. 13688)
SH-66 NB over Bird Creek
Rogers County, Oklahoma
ODOT Construction JP #20899(09)

ODOT Engineering Contract Number: 1902A
JP #33291(04)



May 2022

Report Prepared By: CONSOR Engineers, LLC.

Design Support for Section 4(f) Analysis for Historic Bridges

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Gregg A. Hostetler, P.E.

May 12, 2022
Date



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Introduction

This report was originally submitted to ODOT on March 23, 2020. A revised report was submitted to ODOT on September 9, 2021. This most recent revision to the report, submitted on May 9, 2022, addresses comments received from ODOT on April 9, 2022, and eliminates all references to the terms “Structurally Deficient” and Functionally Obsolete” to comply with FHWA terminology. This latest update also addresses the current condition of the bridge based on the most recent NBIS inspection report and a special inspection completed by another consultant on March 5, 2022.

Section 4(f) regulations (23 CFR 774) state that FHWA (Federal Highway Administration) may not approve an action that uses public park and recreation land, or historic properties, when there is a feasible and prudent alternative to the action. In most cases, actions that use an historic bridge are those that result in demolition/removal of the historic structure or that reconstruct it to such an extent that the character defining features that give it historic significance are eliminated or substantially impaired.

To simplify the 4(f) process, FHWA has established a nationwide Programmatic 4(f) evaluation for historic bridges that specifies a limited set of avoidance alternatives that must be evaluated and rejected before an action that uses an historic bridge can be approved. Programmatic 4(f) evaluations also expedite the 4(f) process because they are approved at the state level by FHWA Division Offices without national legal sufficiency review. To reject an avoidance alternative, FHWA must demonstrate that it cannot be constructed as a matter of sound engineering practice (not feasible) and that it is not a reasonable expenditure of public funds (not prudent). This evaluation must be made in light of the preservation intent of the law and the definition of “feasible and prudent avoidance alternative” in 23 CFR 774.17. If an avoidance alternative exists that is both feasible and prudent, it must be selected by FHWA.

ODOT and FHWA will assess the feasibility and prudence of avoidance alternatives based in part on the information generated in this report. This information may also be used by the agencies to evaluate and incorporate measures to minimize harm resulting from use of an historic bridge that cannot be avoided.

Existing Conditions¹

Located in Rogers County, near the City of Catoosa, the bridge carrying two lanes of SH-66 NB over Bird Creek (Structure No. 6602 0368 EX, NBI No. 13688) is a 824'-8" long, six-span bridge with the following configuration (south to north):

- Span 1 – 102'-8" riveted 5-panel Camelback Pony Truss
- Span 2 – 142'-5" riveted 7-panel Pennsylvania K-Truss

¹ The bridge information included in this section is taken from original bridge plan sheets and standards, the November 14, 2019 ODOT Bridge Inspection Report and notes from a field review performed by CONSOR Engineers, LLC in February 2020. Information taken from other documents is referenced separately.

- Span 3 – 212'-5" riveted 10-panel Pennsylvania K-Truss
- Span 4 – 162'-5" riveted 8-panel K-Truss
- Span 5 – 102'-1" riveted 5-panel Camelback Pony Truss
- Span 6 – 102'-8" riveted 5-panel Camelback Pony Truss

The bridge roadway horizontal clearance is 30'-0" curb-to-curb. The approach roadway width is 37'-1", and the vertical clearance beneath the portal bracing is 15'-6". The bridge was built in 1956 utilizing modified state design standards, which are now obsolete. The spans are supported by two-column reinforced concrete piers with reinforced concrete abutments at each end of the bridge. See Figures 1 and 2 for a location map and vicinity map of the bridge respectively, and Figures 3 through 18 for photos of the existing bridge, at the end of this section of the report.

The posted speed for the roadway at the bridge, classified as a suburban principal arterial, is 45 miles per hour. The current Average Annual Daily Traffic (AADT) is 7,900 vehicles per day²; the future AADT is 11,000 vehicles per day in the year 2040³.

The bridge is in poor condition due to the superstructure having an NBI Condition Rating of 4 (poor condition). In addition to being in poor condition, the bridge has a vertical clearance of 15'-6". Current AASHTO standards require a minimum vertical clearance of 16'-0", and ODOT standards require 16'-9". The bridge, however, shows no signs of impact damage based on the field assessment, and has no instances of impact damage recorded in bridge inspection reports since 2014.

The following information is from the March 5, 2022 "Other Special" bridge inspection report (Refer to Appendix F): The bridge superstructure is in poor condition (NBI Rating = 4) due to severe corrosion and section loss of the floor beams and stringers, active fatigue cracks in stringer copes, floor beam sweep at some locations, and section loss to the floor bracing system. Some of these conditions have worsened significantly since the November 3, 2021 "Routine/Fracture Critical" bridge inspection. The truss upper chords, web members, and end posts are in fair to satisfactory condition. The substructure is in satisfactory condition (NBI Rating = 6) and the deck is in fair condition (NBI Rating = 5).

The deck was rehabilitated in 1979, which included widening the roadway to 30-ft. The structure was painted in 2014⁴ and the deck expansion joints were rehabilitated in 2015⁵. These recent projects were completed with the expectation of extending the service life of the bridge. The paint is in good to fair condition and has effectively arrested active corrosion on portions of the trusses above the deck. The paint is in poor to fair condition below the deck with areas of active corrosion and severe section loss. The rehabilitated pourable expansion joint seals at the

² Traffic Data, ODOT, 2018

³ November 14, 2019 ODOT Bridge Inspection Report

⁴ ODOT Construction Plans – State Job No. 20926(04), U.S. Highway No. 66, Division 8 Joint Project, Rogers County

⁵ ODOT Construction Plans – State Job No. 20926(05), U.S. Highway No. 66, Division 8 Paint Project, Rogers County

piers are in poor condition with adhesion loss and holes in the sealant. The cumulative effect of these preservation activities has extended the service life of the bridge; however, these activities did nothing to address the poor condition of the superstructure or to restore load-carrying capacity.

The bridge is not currently posted for a weight restriction. Based on the 2014 load rating analysis, with modifications accounting for current roadway width, floor beam sweep reported in the November 2021 Inspection Report, and ratings for emergency vehicles (EV-3) and special hauling vehicles (SHV), the bridge does not require posting; however, worsening conditions will likely soon require load posting of the bridge (Refer to Appendix B). All photographs in this report (Figures 3 through 28) were taken during a field review in February 2020.

The trusses (bottom chords, verticals, and diagonal members in tension) and floor beams are classified as fracture critical members; defined as a steel member in tension, or with a tension element, whose failure would probably cause a portion of or the entire bridge to collapse⁶.

Distinguishing Characteristics That Convey Historic Significance

The bridge has been listed on the National Register of Historic Places (NRHP) as a key resource holding intrinsic value in this section of the Route 66 National Scenic Byway. It is considered a bridge with “High” historic significance under Criterion A.

While all truss members are type defining, not all are equally important to conveying historic significance. The distinguishing characteristics that convey historic significance are linked to historic context, particularly the technological context.

The bridge was built in 1956 with a 28-ft wide roadway. When the deck was rehabilitated to facilitate a 30-ft wide roadway, the deck represented late-1950s geometric design modified for state highway standards of the late 1970s. Other than the wider roadway and corresponding floor system, the truss configuration was designed to be identical to the southbound structure built in 1936. At this time, the bridges were dedicated to Henry Tom Kight, Jr., who served as Claremore City attorney, state senator, state highway commissioner, and state highway chairman. The two structures later became affectionately known as the “Twin Bridges” or “Sister Bridges.” The waterway where SH-66 crosses Bird Creek was originally a branch of the Verdigris River. In 1967, the Kerr-McClellan navigation channel was dug out for the Tulsa Port of Catoosa. The 1936 structure was closed to public traffic in 2010 and replaced in 2012; the original spans have been placed on supports within Rogers Point Park.

The distinguishing characteristics that convey the historical significance of the bridge are the truss spans and the construction details which emulated the original 1936 construction; rigid connections, use of I-shapes, and built-up members. Maintaining the design of all truss spans and in-kind replacement of members, meaning mechanical connections and use of I-shape steel sections, will preserve the distinctive characteristics that convey the historical significance of

⁶ 23 CFR Part 650, National Bridge Inspection Standards

the bridge. Adding material to strengthen the bridge, or making needed repairs to deteriorated portions of members, should not adversely affect the bridge as the distinguishing characteristics will remain.

Features or elements that are not distinctive characteristics are the flooring system members, provided that how they are connected to the trusses is maintained. It is important to connect the wider in-kind replacement floor beams, brackets, and bracing using mechanical connections. This way, the truss designs and truss lines, the key distinctive elements of the bridge, will be preserved and remain in use. Placing traffic barriers that meet current safety requirements is also a reversible addition that does not alter the distinguishing characteristics, but does obscure the view of the trusses from the roadway.

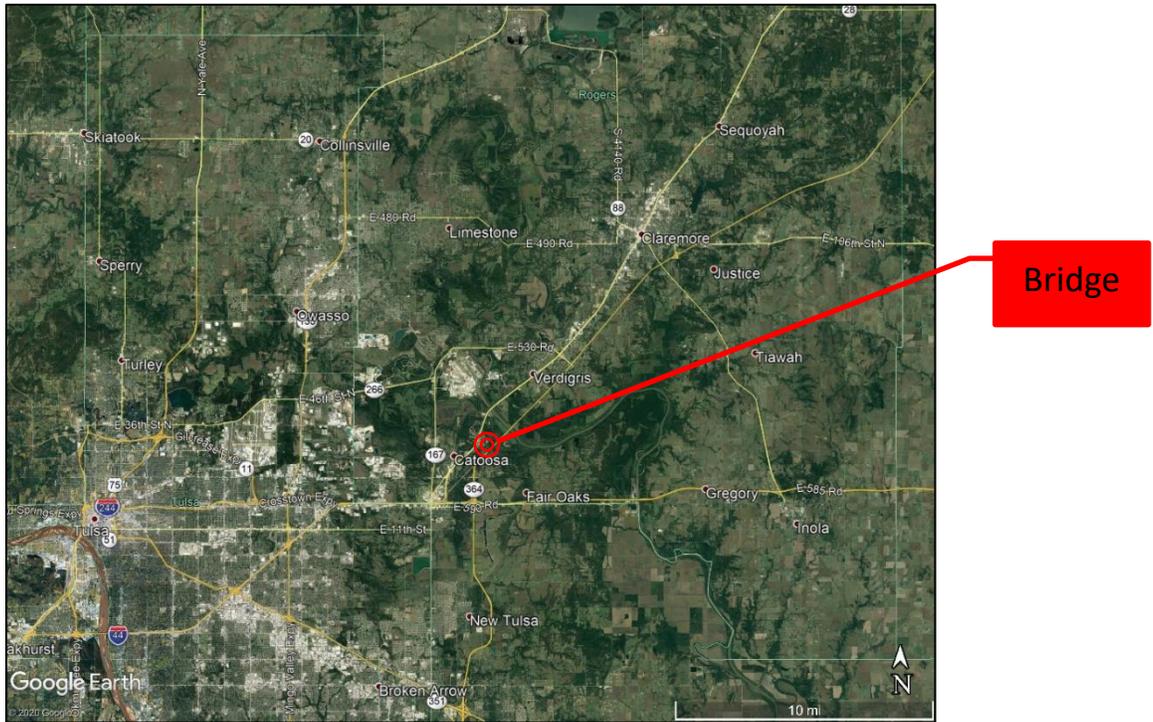


Figure 1: Bridge location map



Figure 2: Bridge vicinity map



Figure 3: West elevation



Figure 4: East elevation



Figure 5: Main span elevation, looking west (span 3)



Figure 6: Span 5 elevation, looking west (Typical pony truss span configuration)



Figure 7: South approach looking north



Figure 8: South approach vertical clearance sign



Figure 9: Channel view looking east (downstream)



Figure 10: Channel view looking west (upstream)



Figure 11: Abutment 1 (south) and underside of Span 1 (looking south)



Figure 12: Abutment 2 (north) and underside of Span 6 (looking north)



Figure 13: Typical underside configuration of main span (span 3)



Figure 14: Pier 2, looking south (typical pier configuration)



Figure 15: Pier 2 bearings (typical expansion bearing configuration)



Figure 16: Abutment 2, east bearing, typical fixed bearing configuration



Figure 17: Monument plaque at southeast corner



Figure 18: Typical floor beam configuration at pier



Figure 19: Pier 4, floor beam bottom flange; section loss and welded repair angle



Figure 20: Pier 3, floor beam bottom flange; reactivated corrosion and section loss



Figure 21: Span 3, floor beam 0 at the east truss; section loss and perforation with active corrosion adjacent to connection angles



Figure 22: Typical welded floor beam repair plates

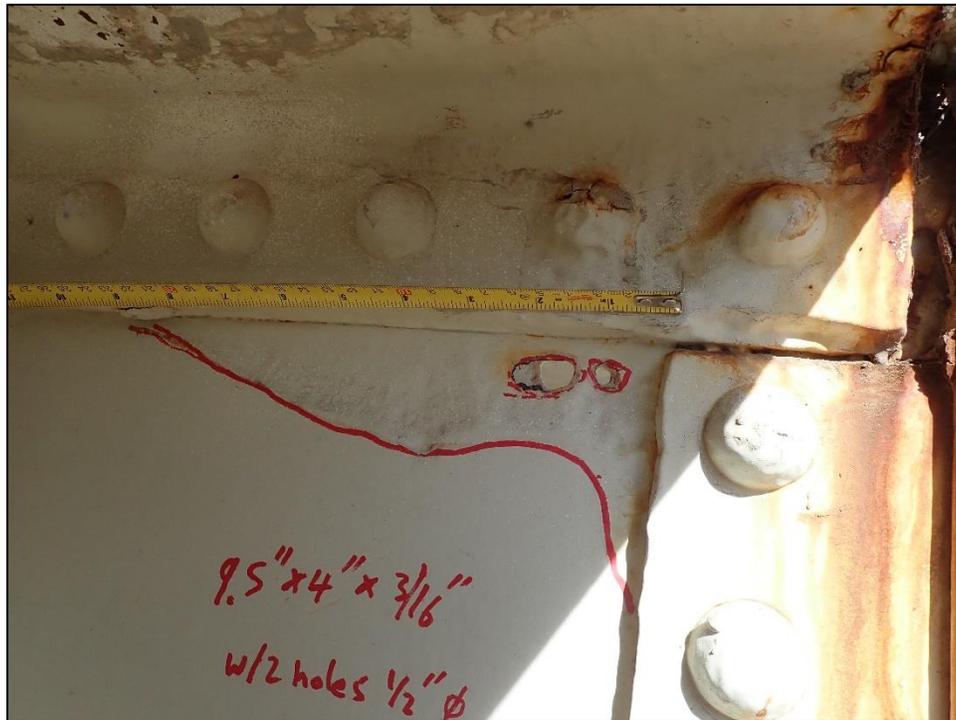


Figure 23: Span 4, floor beam 6, east end; corrosion holes up to 2-in diameter with adjacent painted-over tapered section loss



Figure 24: Span 6; stringer 4 at floor beam 4; painted over pitting
Typical condition for interior stringer to interior floor beam connections



Figure 25: Span 1, floor beam 1, west end; section loss up to ¼-in deep



Figure 26: Span 5, east truss at floor beam 5; severed lower lateral bracing



Figure 27: Span 6, west truss, lower chord web; section loss with knife-edging along inboard web plate



Figure 28: Typical active corrosion inside lower chord panel points

Purpose & Need for this Project

The following purpose and need for this project were provided by ODOT:

The purpose of this project is to provide a safe crossing and preserve transportation continuity over Bird Creek. The need of this project is to address the current structural and functional deficiencies of the existing bridge and approach roadway.

Alternatives Analysis

Alternatives that would avoid replacement of the existing bridge have been evaluated to determine probable costs and the extent of work required to satisfy the project purpose and need. To that end, the following alternatives have been evaluated:

- Alternative 1: Do Nothing
- Alternative 2: Rehabilitation Without Affecting Historic Integrity of the Bridge
 - Alternative 2(a): Rehabilitation existing bridge; widen to provide 38-foot roadway
 - Alternative 2(b): Rehabilitation of existing bridge; maintain current 30-foot roadway width
- Alternative 3: Build on New Location: Retain existing bridge in place, either as a non-functional “monument” or as a non-motorized pedestrian or bicycle facility
- Alternative 4: Replacement of bridge on existing alignment

All analyses have been performed in accordance with the American Association of State Highway and Transportation Officials (AASHTO) Manual for Bridge Evaluation, 2nd Edition and AASHTO Standard Specifications for Highway Bridges, 17th Edition. Models were prepared using the March 2014 load rating, November 2021 fracture critical report, and the field review performed by CONSOR in February 2020. The existing load rating provided by the Division was updated to reflect the current 30-ft roadway (previous calculations were completed using a 28-ft roadway). These new calculations also account for EV-3 and SHV loads.

Alternative 1 - Do Nothing

The do nothing alternative consists of no improvements to correct the poor condition of the bridge or geometrically substandard aspects of the bridge, but would include some minor superstructure repairs to keep the bridge in service. As the bridge continues to age, it will require more frequent inspections and a great frequency of repair and maintenance activities. This includes not only ODOT in-house maintenance work, but contract work for more intensive repair and rehabilitation activities. Because portions of the bridge structure are above the roadway level, the bridge may require periodic closures (lane closures or full bridge closure) to perform needed maintenance such as bolting supplemental plates to strengthen members with section loss or zone-painting. For such closures, existing crossovers north and south of the bridge could be re-used to place 2-way traffic on the westbound bridge.

This alternative has the following advantages:

- Minimal roadway construction and utility impact costs
- Minimal traffic disruptions
- Minimal environmental impacts, including minimal or no adverse effects to the NRHP-eligible bridge
- Minimal engineering costs

This alternative has the following disadvantages:

- Does not address ODOT's purpose and need for the project
- High potential for emergency bridge closures due to continued deterioration
- Periodic bridge closures (lane or complete) for maintenance/repairs
- Many elements not up to current design standards
- Bridge will require load posting
- High annual inspection and maintenance costs

The anticipated effect of this alternative on NBI condition ratings is as follows:

NBI Item #	NBI Item Description	November 2019 Rating	Anticipated Rating
58	Deck	5 - Fair	5 - Fair
59	Superstructure	4 - Poor	4 - Poor
60	Substructure	6 - Satisfactory	6 - Satisfactory

The preliminary construction cost estimate (refer to Appendix A) for this alternative is \$891 thousand. This cost does not include any bridge approach roadway work. The estimated 20-year cost for maintenance and inspection of the bridge for this alternative is approximately \$5.88 million in 2022 dollars.

Alternative 2 - Rehabilitation Without Affecting Historic Integrity of the Bridge

The bridge was constructed in 1956 and rehabilitated in 1979. Design specifications in place at the time of construction are generally considered to provide a 50-year service life⁷, which has since been exceeded. Reconstructed portions are nearing the end of the designed service life. Rehabilitation of the bridge includes the cost of performing repairs, strengthening and replacing bridge components as needed. SH-66 is classified as an urban collector at the bridge site.

To fairly consider rehabilitation, the minimum bridge clear width required by the AASHTO Green Book⁸ must be considered. Following are some key Green Book considerations:

- Chapter 6: Collector Roads and Streets
 - Section 6.3.3.1: “For collector streets with shoulders and not curbs, the full width of approach roadways should preferably be extended across bridges.” “Table 6-6 applies to bridge widths on urban collector streets.
 - Table 6-6: For design volume (veh/day) over 2,000, the minimum clear roadway width for new and reconstructed bridges should be equal to the approach roadway width (surfaced traveled way plus shoulders). A footnote states the following: “For bridges in excess of 100 ft in length, the minimum width of traveled way plus 3 ft on each side is acceptable.”

Currently, the approach roadway at each end of the existing bridge is approximately 37-ft wide. Therefore, a 38-ft clear bridge width is recommended to meet the Green Book criteria shown above. The existing bridge clear width of 30 ft does, however, comply with the Green Book minimum clear width criteria for bridges over 100 ft in length, in that the travel way width of 24 ft plus 3 ft on each side equals 30 ft. Therefore, a design exception is not required to rehabilitate the bridge without widening.

The following options are considered within this alternative:

- 2(a) - Rehabilitation of existing bridge; widen to provide 38-ft roadway
- 2(b) - Rehabilitation of existing bridge; maintain current 30-ft roadway width

⁷ Bridge Preservation Guide, FHWA, August 2011.

⁸ A Policy on Geometric Design of Highways and Streets, AASHTO, 7th Edition, 2018. This publication is commonly referred to as the “AASHTO Green Book.”

Alternative 2(a) – Rehabilitation of existing bridge; widen to provide 38-ft roadway

Analysis of this alternative reveals that widening the bridge cannot be completed without adversely affecting the historic integrity of the structure.

To provide a clear roadway width of 38-ft, the deck, stringers, floor beams, and upper lateral bracing system require replacement. The deck, stringers and floor beams are not considered to be character defining features of the historic bridge. They can be replaced without having an adverse effect on the structure’s historic significance. The primary truss members could remain⁹, but must be moved to accommodate the wider roadway section. The bracing system, however, would need to replacement, creating an adverse effect on the structure’s historic significance due to the change in geometry.

Using the existing BAR7 load rating files provided by the Division, the additional live load and dead load effects due to the widening were approximated and input to analyze the truss members (Refer to Appendix C). This analysis assumes the deck and floor system would be a new construction, would be designed to sufficiently handle the new geometry, and would incur an increase in dead load to be carried by the truss members proportional to the widening of the deck.

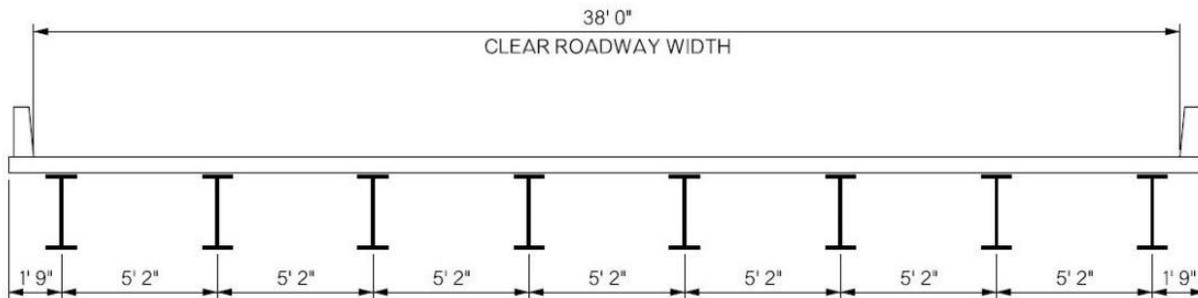


Figure 31: Typical Section: Widened Bridge

⁹ Guidelines for Historic Bridge Rehabilitation and Replacement, AASHTO, November 2008.

The increased loads from the proposed widening (preferably to one side) of the bridge result in the following members being overstressed (indicated in red). See Figures 33-36. Note that this analysis did not include gusset plates. Additional analysis would be required to verify gusset plate sufficiency.

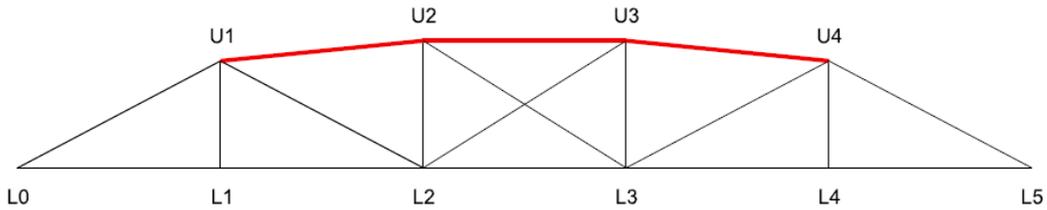


Figure 32: Overstressed Truss Members (Spans 1, 5, and 6) – Rehabilitation with Widening

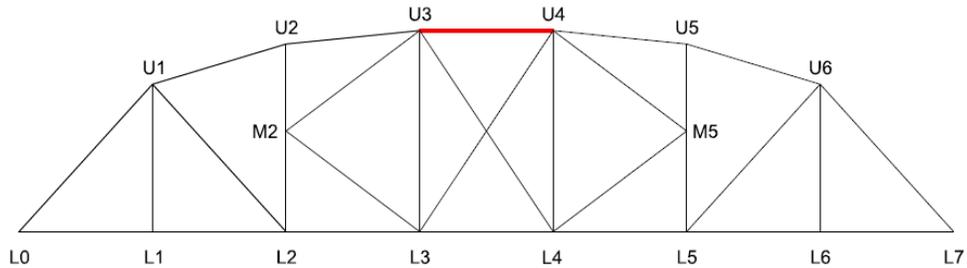


Figure 33: Overstressed Truss Members (Span 2) – Rehabilitation with Widening

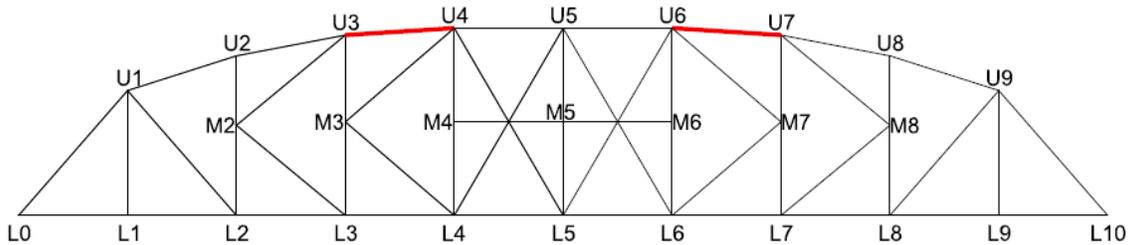


Figure 34: Overstressed Truss Members (Span 3) – Rehabilitation with Widening

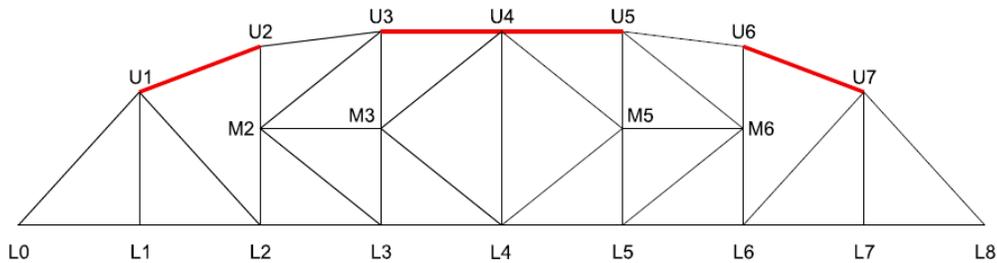


Figure 35: Overstressed Truss Members (Span 4) – Rehabilitation with Widening

The top chords are limited by buckling of the built-up sections. They can be strengthened by bolting a plate between the back-to-back channel flanges or bolting additional material to the channel webs.

Both abutments and all piers require modification (augmentation) to accommodate the widened portion of the structure. The anticipated substructure modifications include several new steel H-piles at each abutment and one new drilled shaft at each pier. The substructure work should be designed and constructed to closely match the look of the existing abutments and piers. Nevertheless, these modifications may be deemed an adverse effect, as the substructure is clearly visible from the parallel structure.

Additional recommended bridge work for this alternative includes the following:

- Reset rocker bearings
- Upgrading the roadway barriers to meet current criteria. The new railings should be designed to be attached to the new stringers, new floor beams, or new deck and have a similar open look to the original railing. AASHTO LRFD Design Specifications includes a discussion of the types of loads that a railing should be designed to. Crash-tested and approved railing types and configurations can be found on the FHWA Safety website for bridge railings:
https://safety.fhwa.dot.gov/roadway_dept/countermeasures/reduce_crash_severity/listing.cfm?code=long
- Clean and paint all existing structural steel that is to remain. The bridge was painted in 2014, and it is unlikely to be lead-based; special containment (Class 1A) would most likely not be required. A coatings specialist (NACE Level III or BCI Level II Certified) should be retained to perform the tests and make recommendations for the required work. In order to ensure the best quality paint application, the existing steel should be cleaned to bare metal per Structural Steel Painting Council (SSPC) Standard SP10, Near White Blast Cleaning. It should be noted that the presence of hazardous metals in the paint system should be verified and appropriate steps should be taken to ensure a safe environment for workers during removal.
- Substructure repairs
- Replacement of both approach slabs

Following is a possible sequence to perform the work:

1. Close roadway and remove each truss span after flooring system is removed. Provide level truss lay-down area adjacent to bridge or transport to steel fabrication facility. Make necessary modifications to trusses and gusset plates.
2. Install piles and drilled shafts for widened abutments and intermediate piers
3. Widen abutments and piers by splicing into existing structure
4. Reset rocker bearings
5. Reinstall trusses
6. Install new flooring system (stringers and floor beams)
7. Place new deck, approach slabs, and traffic rails
8. Reopen bridge to traffic

Crane access appears to be available at the northeast or southwest quadrants of the bridge with appropriate clearing, grubbing, and leveling of the area. Some additional fill and/or stabilization may be required to support the crane. The adjacent structure could be utilized with traffic closure.

The anticipated effect of this alternative on several key bridge ratings/indicators is as follows:

NBI Item #	NBI Item Description	November 2019 Rating	Anticipated Rating
58	Deck	5 - Fair	8 - Very Good
59	Superstructure	4 - Poor	6 - Satisfactory or 7 - Good
60	Substructure	6 - Satisfactory	6 - Satisfactory or 7 - Good

This alternative has the following advantages:

- Bridge is no longer in poor condition
- Bridge width is equivalent to the approach roadway width
- Bridge no longer requires load posting
- Historic integrity of truss members is maintained

This alternative has the following disadvantages:

- Adverse effect determination is likely, but strong mitigation option
- Some environmental impacts
- Bridge remains fracture critical, relying on steel tension members which are nearing or have already exceeded the design fatigue life.
- Traffic impacted during construction

The preliminary construction cost estimate for this alternative (refer to Appendix A) is approximately \$10.05 million. This cost does not include any bridge approach roadway work. The estimated 20-year cost for maintenance and inspection of the bridge for this alternative is approximately \$1.67 million in 2022 dollars.

Alternative 2(b): Rehabilitation of existing bridge; maintain current 30-ft roadway width

With this alternative, the bridge would have sub-optimal shoulder widths and continue to provide a clear roadway width (30-ft) that is less than the approach roadway width (37-ft).

To ensure ample reserve capacity of the floor system, the stringers and floor beams should be replaced with new members using higher strength steel. The floor system replacement will also require replacement of the deck. Preliminary analysis indicates that use of 50 ksi yield strength steel will allow the floor beams and stringers to be replaced with members of comparable depth, thus retaining the visual appearance of the truss span. These members can be replaced with no adverse effect on the truss span, as discussed in Alternative 2(a). The truss members are not deficient in this regard, although an in-depth analysis of the gusset plates should be performed during the design phase.

Jacking the trusses will be required to reset expansion bearings. This can be done either from locations on the piers/abutments or using temporary supports braced to the existing piers. Analysis will be required to determine the suitability of either method, or if a different method will be required.

Additional recommended bridge work for this alternative includes the following:

- Upgrading the roadway barriers to meet current criteria. The new railings should be designed to be attached to the new stringers, new floor beams, or new deck and have a similar open look to the original railing. AASHTO LRFD Design Specifications includes a discussion of the types of loads that such a railing should be designed for. Crash-tested and approved railing types and configurations can be found on the FHWA Safety website for bridge railings:
https://safety.fhwa.dot.gov/roadway_dept/countermeasures/reduce_crash_severity/listing.cfm?code=long
- Clean and paint all existing structural steel that is to remain. The bridge was painted in 2014, and it is unlikely to be lead-based; special containment (Class 1A) would most likely not be required. A coatings specialist (NACE Level III or BCI Level II Certified) should be retained to perform the tests and make recommendations for the required work. To ensure the best quality paint application, the existing steel should be cleaned to bare metal per Structural Steel Painting Council (SSPC) Standard SP10, Near White Blast Cleaning. It should be noted that the presence of hazardous metals in the paint system should be verified, and appropriate steps should be taken to ensure a safe environment for workers during removal.
- Substructure repairs
- Replacement of both approach slabs

Following is a possible sequence to perform the work:

1. Close bridge; remove existing deck and floor system
2. Replace or strengthen truss members and gusset plates as required
3. Jack trusses and reset rocker bearings
4. Erect new flooring system (stringers and floor beams)
5. Clean and paint bridge
6. Place new deck, approach slabs, and traffic rails
7. Reopen bridge to traffic

Crane access for this alternative is the same as specified in Alternative 2(a).

The anticipated effect of this alternative on several key bridge ratings/indicators is as follows:

NBI Item #	NBI Item Description	November 2019 Rating	Anticipated Rating
58	Deck	5 - Fair	8 - Very Good
59	Superstructure	4 - Poor	5 - Fair or 6 - Satisfactory
60	Substructure	6 - Satisfactory	6 – Satisfactory or 7 - Good

This alternative has the following advantages:

- Bridge is no longer in poor condition
- Bridge no longer requires load posting
- No adverse effects

This alternative has the following disadvantages:

- Bridge will remain geometrically substandard
- Some environmental impacts
- Bridge remains fracture critical, relying on steel tension members which are nearing or have already exceeded the design fatigue life.
- Traffic impacted during construction

The preliminary construction cost estimate (refer to Appendix A) for this alternative is approximately \$7.60 million. This cost does not include any bridge approach roadway work. The estimated 20-year cost for maintenance and inspection of the bridge for this alternative is approximately \$1.18 million in 2022 dollars.

Alternative 3 - Build on New Location

This alternative analyzes keeping the bridge in place, either as a non-functional “monument” or as a non-vehicular pedestrian or bicycle facility. A new bridge would be built on an offset alignment. Due to the level of service of SH-66 in the vicinity of the Bird Creek crossing, and proximity to bridges that would prohibit use, pedestrian and bicycle access would not likely be utilized. Therefore, evaluation utilizing AASHTO pedestrian bridge guidance¹⁰ is not required.

The scope of work for this project does not include an in-depth assessment of any new build alignments. However, the March 2011 analysis¹¹ submitted before the replacement of the twin structure explored several different offset alignment alternatives, as well as the advantages and disadvantages of each alternative. The report included 6 different alignment options (Build Alternatives A-F), with offsets on both sides of the pair of structures. The result of this analysis is likely still valid for determination of feasibility and prudence for building on a nearby offset location, and an excerpt of the report is included in Appendix H.

All six of the offset alignment alternatives are feasible and meet the purpose and need of this project. However, all would result in adverse impacts to the historic integrity of the bridge, Rogers Point Park, or both. Each of these offset alternatives are constrained by the need to safely tie into the existing Verdigris River Navigation Channel bridges located approximately 1700-ft north-northeast of the Bird Creek crossing while maintaining a safe and acceptable horizontal and vertical roadway geometry. The original analysis determined an approximate requirement of new right-of-way (R/W), what extent this R/W acquisition would impact Rogers Point Park, and an approximate cost to engineer the required superelevation and alignment relative to the cost of maintaining the current alignment.

¹⁰ LRFD Guide Specifications for the Design of Pedestrian Bridges, AASHTO, December 2009.

¹¹ Programmatic Section 4(f) Evaluation on Structure # 6602 0368WX, BRFY-166C(130), J/P 20889(04), Rogers County Oklahoma. Prepared by ODOT Environmental Programs Division. March 2011.

The following table summarizes these alternatives as they would apply to the scope of this analysis.

Build Alternative	R/W Required	Rogers Point Park Impact	Relative Cost
A – Construct new bridge on an offset 70’ east of the existing east/north bound bridge.	9 acres	< 1 acre	175%
B – Construct new bridge on an offset 120’ east of the existing east/north bound bridge. ^a	-	-	-
C – Construct new bridge on an offset 70’ to the west of existing west/south bound bridge. East/north bound traffic would be re-routed to the current west/south bound bridge and the west/south bound traffic would be re-routed to the newly constructed bridge. ^b	11.3 acres	4.1 acres	176%
D – This alternative involved building a new 4-lane structure and is no longer applicable.	-	-	-
E – Construct new bridge on an offset 120’ west of current west/south bound bridge. East/north bound traffic would be re-routed to the current west/south bound bridge and the west/south bound traffic would be re-routed to the newly constructed bridge. ^b	11.3 acres	4.1 acres	188%
F – Construct new bridge 50’ to west of current west/south bound bridge. East/north bound traffic would be re-routed to the current west/south bound bridge and the west/south bound traffic would be re-routed to the newly constructed bridge.	13.8 acres	4.1 acres ^c	

^a Such a significant offset is substandard and results in an unsafe alignment.

^b Additionally, this alternative may require acquisition and relocation of a residence southwest of the bridge and two commercial properties west of SH-66.

^c This acquisition includes the only park access from the west/south bound lanes of SH-66.

This alternative has the following advantages:

- Fracture critical bridge removed from vehicular service
- Historic integrity of bridge is maintained with regard to character-defining features

This alternative has the following disadvantages:

- Adverse effect due to bridge no longer carrying traffic on Route 66
- Transfer of bridge ownership may be a challenge
- Bridge still requires routine maintenance

- Additional cost for offset alignment
- Some environmental impacts
- Right-of-way acquisition costs/impacts

For use as a monument, steps need to be taken to prevent public access to the bridge. The cost to secure the bridge from the public using fencing and barricades or guardrail, and related activities, is relatively small and has not been estimated as part of this design analysis.

However, the bridge will continue to deteriorate, and some maintenance will be required. If simple maintenance is not performed, the structure will become a liability to the owner. The estimated 20-year cost for maintenance and inspection of the bridge is \$338 thousand in 2022 dollars. The estimated cost for construction of the new bridge on an offset alignment is \$6.9 million.

Alternative 4: Replacement of Bridge on Existing Alignment

For this alternative a new bridge will be constructed on the existing alignment. As a mitigation measure, the existing trusses can be transported to a nearby location to be placed on display on pedestals, like the original westbound structure. To estimate the cost of this alternative, a configuration like that used for the 2012 construction of the adjacent structure was assumed. Standards should be utilized in the design to the extent possible.

This alternative has the following advantages:

- New bridge that fulfills the purpose and need of the project
- Elimination of a bridge that is in poor condition, load posted, and fracture critical bridge
- Significantly reduced maintenance costs

This alternative has the following disadvantages:

- This is not an "Avoidance Alternative"
- Some environmental impacts
- Traffic impacted during construction of new bridge and approach roadway

The preliminary cost estimate (refer to Appendix A) for this alternative is \$7.25 million. This cost does not include any bridge approach roadway work. The estimated 20-year cost for maintenance and inspection of the bridge is \$196 thousand in 2022 dollars.



Figure 36: Spans 2 and 4 from 1939 bridge on display at Rogers Point Park flanking access road northeast of structure at Rogers Point Park



Figure 37: Spans 2 and 4 from 1939 bridge on display at Rogers Point Park flanking access road northeast of structure at Rogers Point Park



Figure 38: Span 3 from original 1939 bridge on display at Molly's Landing southwest of Bird Creek Crossing

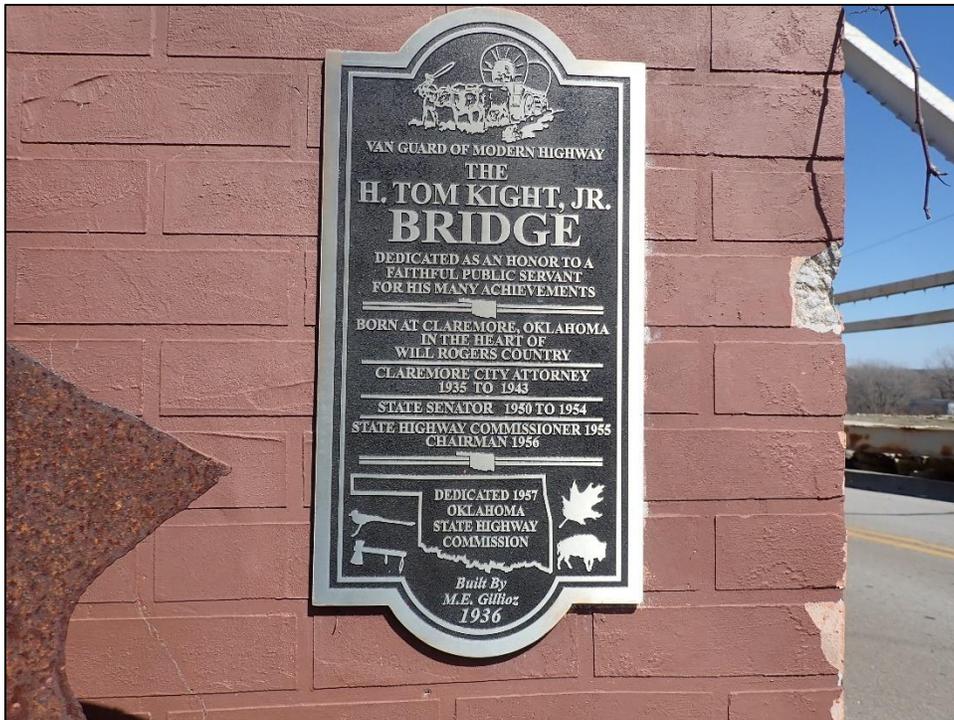


Figure 39: Dedicated plaque on display at Molly's Landing

Summary of Findings

Category	Avoidance Alternatives				#4: New Bridge on Existing Location
	#1: Do Nothing	#2: Rehabilitation		#3: Build on New Location, Leave in Place as Monument	
		#2(a): Widen	#2(b): No Widening		
Maintenance and Inspection	<ul style="list-style-type: none"> Increased inspection frequency Increased frequency of repairs to address section loss in steel, particularly stringers and floor beams Increased frequency of maintenance required, including spot painting 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Structural repairs should not be required if regular program of cleaning the trusses and spot painting areas of corrosion is initiated 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Structural repairs should not be required if regular program of cleaning the trusses and spot painting areas of corrosion is initiated 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Significantly reduced inspection frequency and effort 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-30 years Inspection effort significantly reduced; new bridge and no longer fracture critical
Geometric Adequacy	<ul style="list-style-type: none"> Bridge remains geometrically substandard 	<ul style="list-style-type: none"> Bridge geometry improved to comply with AASHTO Green Book Provides 38 feet clear roadway width 	<ul style="list-style-type: none"> Bridge remains geometrically substandard 	<ul style="list-style-type: none"> Not applicable as the bridge is no longer open to vehicular traffic 	<ul style="list-style-type: none"> Bridge meets current AASHTO and ODOT geometric standards
Structural Adequacy	<ul style="list-style-type: none"> Remains in poor condition Remains Fracture Critical Load posting is likely in near future 	<ul style="list-style-type: none"> No load posting required No longer in poor condition Remains Fracture Critical 	<ul style="list-style-type: none"> No load posting required No longer in poor condition Remains Fracture Critical 	<ul style="list-style-type: none"> Monument use requires fencing or other means to keep public off bridge, while allowing access for maintenance vehicles Not applicable as the bridge is no longer carrying vehicles 	<ul style="list-style-type: none"> Bridge that is in poor condition and Fracture Critical is removed from service

Category	Avoidance Alternatives				#4: New Bridge on Existing Location
	#1: Do Nothing	#2: Rehabilitation		#3: Build on New Location	
		#2(a): Widen	#2(b): No Widening		
Environmental	<ul style="list-style-type: none"> • Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel 	<ul style="list-style-type: none"> • Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel • Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> • Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel • Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> • Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel • Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> • Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel • Jurisdictional Waters and Wetlands
Permits	<ul style="list-style-type: none"> • None Anticipated 	<ul style="list-style-type: none"> • US Army Corps of Engineers – Nationwide 14 • Flood Plain Permit (County) • DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> • US Army Corps of Engineers – Nationwide 14 • Flood Plain Permit (County) • DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> • US Army Corps of Engineers – Nationwide 14 • Flood Plain Permit (County) • DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> • US Army Corps of Engineers – Nationwide 14 • Flood Plain Permit (County) • DEQ OK R10 (Construction Stormwater Permit)
Adverse Effects on Historic Bridge	<ul style="list-style-type: none"> • None Anticipated 	<ul style="list-style-type: none"> • Rebuilt bracing system • Potentially adverse effect due to change in clear width and substructure modification • Modifications to floor system and truss members not anticipated to have adverse effect 	<ul style="list-style-type: none"> • Modifications to floor system and truss members not anticipated to have adverse effect • Bridge retains appearance and function 	<ul style="list-style-type: none"> • Effect determination will require consultation with SHPO – expected that work will not cause an adverse effect to the character defining features of the bridge, but change of use will likely be an adverse effect due to bridge being on historic route 	<ul style="list-style-type: none"> • Truss spans could be placed on display near the surrounding area like the previous westbound bridge
Construction Cost (Bridge Only)	\$ 891 thousand	\$ 10.05 million	\$ 7.60 million	\$ 10 thousand for existing bridge; \$6.9 million for new bridge	\$ 7.25 million
20-Year Maintenance & Inspection Cost (2022 Dollars)	\$ 5.88 million	\$ 1.67 million	\$ 1.18 million	\$ 338 thousand	\$ 196 thousand

Works Cited

Other Special Bridge Inspection Report, NBI No. 13688, Structure No. 6602 0368 EX, Oklahoma Department of Transportation, Inspection Date March 5, 2022

Bridge Inspection Report, NBI No. 13688, Structure No. 6602 0368 EX, Oklahoma Department of Transportation, Inspection Date November 3, 2021

ODOT Construction Plans – State Aid Project No. 183(6), U.S. Highway No. 66, Rogers County, Sheets 5 through 7 (Revised September 14, 1955)

ODOT Construction Plans – State Job No. 20926(04), U.S. Highway No. 66, Division 8 Paint Project, Rogers County, Sheets 1, 2, and 8 (Revised March 14, 2013)

ODOT Construction Plans – State Job No. 20926(05), U.S. Highway No. 66, Division 8 Joint Project, Rogers County, Sheet 1 (Revised May 14, 2015)

ODOT Construction Plans – State Job No. 20899(04), U.S. Highway No. 66, Bridge & Approaches, Rogers County, Sheets 1, 10 (Revised June 27, 2011), 19, 20 (Revised June 27, 2011), 35 through 42, 49 (Revised June 27, 2011), and 53 (Revised June 27, 2011)

A Policy on Geometric Design of Highways and Streets, American Association of State Highway and Transportation Officials, 7th Edition, 2018

National Bridge Inspection Standards (NBIS), 23 CFR Part 650

Bridge Preservation Guide – Maintaining a Good State Using Cost Effective Investment Strategies, Federal Highway Administration, Publication Number FHWA-HIF-11042, August 2011

Manual for Bridge Evaluation, American Association of State Highway and Transportation Officials, 2nd Edition, 2011 with interims through 2016

Standard Specifications for Highway Bridges, American Association of State Highway and Transportation Officials, 17th Edition, 2002

Guidelines for Historic Bridge Rehabilitation and Replacement, American Association of State Highway and Transportation Officials, November 2008

LRFD Guide Specifications for the Design of Pedestrian Bridges, AASHTO, December 2009

Programmatic Section 4(f) Evaluation on Structure # 6602 0368WX, BRFY-166C(130), J/P 20889(04), Rogers County Oklahoma. Prepared by ODOT Environmental Programs Division. March 2011.

Appendix A

Preliminary Construction and Maintenance Cost Estimates

Appendix B

Load Rating Analysis Results

Appendix C

Alternative 2(a) Load Rating Analysis Results (38-foot Roadway)

Appendix D

Alternative 2(b) Load Rating Analysis Results (30-foot Roadway)

Appendix E

Additional Field Review Photographs

Appendix F

November 2021 and March 2022 Bridge Inspection Reports

Appendix G

Select Plan Sheets and Obsolete ODOT Bridge Standards

Appendix H

Excerpt from March 2011 Section 4(f) Analysis



Oklahoma Historical Society

Founded May 27, 1893

State Historic Preservation Office

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

October 19, 2021

Mr. Scott Sundermeyer, Director
ODOT Cultural Resources Program
3200 Marshall Avenue, Room 110
Norman, OK 73019

RE: File #3071-21 [Previously #0562-20]; SH-66 Bridge over Bird Creek Proposed Improvements Project, #JP-20899(09), Rogers County

Dear Mr. Sundermeyer:

We have received and reviewed the documentation on the referenced project with your letter dated September 28th. The historic property to be impacted is the Verdigris River Bridge aka Bird Creek Bridge #6602-0368-EX, a property that has been previously determined eligible for listing in the National Register of Historic Places (NRHP) under Criterion C for Engineering. Please note, the Design Support for Section 4(f) Analysis Report for this bridge structure prepared by CONSOR Engineers, LLC, (September 2021), misidentifies this bridge as listed on the NRHP, and misidentifies the historic significance as Criterion A (Page 3).

In your letter and attached report, five (5) alternatives were presented for review and comment. Based on the information and supporting discussion for each alternative, it is the opinion of the Oklahoma State Historic Preservation Office that Alternative 2(b) will have the least adverse effect on the historic bridge while still satisfying the purpose and need of the project and keeping the bridge in use.

Our opinion is based on the fact that Alternative 2(b) proposes to rehabilitate the original, historic, and iconic trusses, and replace the original superstructure (including the road deck, floor beams, and stringers). Although this option will remove a significant amount of original material from the historic structure, which will be an adverse effect, the deck has already been somewhat altered and the proposed replacement will be as “like in-kind” as possible while restoring its structural sufficiency. This alternative maintains the current 30’-0” road width, which will maintain the existing experience at the bridge. Alternative 2(b) also reduces alterations to the historic trusses. Maintaining the trusses as structural components of the bridge also reduces the adverse effect.

We welcome the opportunity to continue consultation to mitigate, minimize, or eliminate the adverse effect of the proposed project. However, if we are unable to eliminate the adverse effect of the project, the Federal Highways Administration (FHWA) will need to contact and invite the participation of the Advisory Council on Historic Preservation (ACHP) to complete the Section 106 process as outlined in 36 CFR Part 800. Should the ACHP choose not to participate in the consultation, FHWA and the SHPO may execute a Memorandum of Agreement (MOA). Upon the execution of an MOA, a copy must be filed with the ACHP to complete the Section 106 process.

Mr. Sundermeyer
October 19, 2021
Page 2

RE: File #3071-21 [Previously #0562-20]; SH-66 Bridge over Bird Creek Proposed Improvements Project, #JP-20899(09), Rogers County

If you have any questions, please contact Sara Werneke, Historic Preservation Architect, at 405/522-4478. Please reference the above underlined file number when responding.

Sincerely,



Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm

September 28, 2021

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Dear Ms. Ozan:

Re: SHPO File no. 0562-20; Rogers County FHWA Project JP 20899(09): Proposed improvements to the historic eastbound bridge carrying SH-66 over Bird Creek; submittal of Design Analysis for comment.

Thank you for your attendance at the Section 106 consulting party meeting on September 20, 2021. As indicated in the meeting, and in prior correspondences, ODOT must consider several alternatives that preserve the historic Bird Creek bridge. The attached document presents these alternatives:

1. Do nothing ["no build" or no action option]
2. Build a new structure at a different location and leave the existing historic bridge in place to function as either a pedestrian bridge or a monument.
3. Rehabilitation without affecting the historic integrity of the bridge.

In addition, ODOT has also presented an alternative to widen the existing bridge and to replace the existing bridge on its current alignment. These are also documented in the attached report.

We are submitting the attached Design Analysis for review and comment. ODOT is continuing through the environmental and design review of alternatives to improve this crossing at Bird Creek. The document is available (with appendices) at <http://www.odotculturalresources.info/bird-creek-bridge.html>. Additional materials are also posted at the website listed above.

If you have any questions regarding this undertaking, please contact me at 405-325-7201.

Sincerely,



Scott Sundermeyer
ODOT Cultural Resources Program Director

cc: State Archaeologist

From: [Scott Sundermeyer](#)
To: [Kelly Saladis](#); [Erin Faulkner](#); [ghostetler](#); [Joe Brutsche](#); [Jennifer Bullard](#); [Jenny Droscher](#); [Justin Hernandez](#); [Katie Brown](#); [Karen Orton - FHWA](#); [Matt Casillas](#); [Nguyen, Ralph \(FHWA\)](#); [Rick Mitchell](#); [Randle White](#); [Sara Downard](#); [Victoria Raines](#); [kitty@historicbridgefoundation.com](#); [rt66theroadahead@gmail.com](#); [Margaret_Frisbie@nps.gov](#); [anneehaaker@gmail.com](#); [jblish@cityofcatoosa.org](#); [rhys.martin@oklahomaroute66.com](#); [Cari Foster](#); [director@preservationok.org](#); [kaisa_barthuli@nps.gov](#); [Lynda Ozan](#); [Kristina Wyckoff](#); [Nathan Holth](#); [John Corpolongo](#); [Anna Eddings](#); [Kimberly Diedrich](#); [Amber McIntyre \(AMCINTYRE@ODOT.ORG\)](#); [carrie.rush@travelok.com](#)
Subject: Rogers County 20899(09) SH-66 Bird Creek Bridge - Virtual Section 106 Consulting Party Meeting video posted
Date: Friday, September 24, 2021 8:26:00 AM
Attachments: [20210920_SH66 over Bird Cr JP 20899\(09\) Section106 Consulting Party meeting minutes.pdf](#)

Good morning all –

Monday's Section 106 consulting party meeting video link and PowerPoint presentation are posted at the project website: <http://www.odotculturalresources.info/bird-creek-bridge.html>. I have also attached a copy of the meeting minutes for your review and comment. If there are no comments to the meeting minutes by Friday, October 1, I will post these minutes to the website as well.

Thank you very much for your attendance at the meeting, and we look forward to comments. Please do not hesitate to reach out to me with any questions.

Best-
Scott

Scott A. Sundermeyer
Cultural Resources Program Director
Oklahoma Department of Transportation
3200 Marshall, Room 110
Norman, OK 73019
Desk: 405-325-7201
Cell: 405-388-2201
ssundermeyer@odot.org



**SH-66 NB over Bird Creek
Rogers County, JP 20899(09)
Virtual Section 106 Consulting Party Meeting Minutes
9/20/2021**

Attendees (virtual):

Erin Faulkner, NEPA PM District 8, ODOT	Jennifer Bullard, ODOT Construction Engineer
Joe Brutsche, Division Manager, ODOT Environmental Programs	Matt Casillas, District 8 Bridge Engineer, ODOT
Scott Sundermeyer, ODOT Cultural Resources Program	Cari Foster, ODOT Cultural Resources Program
Justin Hernandez, ODOT Bridge Engineer	Sara Downard, ODOT PM
Katie Brown, ODOT	Jenny Droscher, ODOT Public Involvement
Thomas Askegaard, ODOT	Gregg Hostetler, CONSOR Engineer
Rick Mitchell, Mead and Hunt	Kelly Saladis, CP&Y
Tori Raines, CP&Y	Kitty Henderson, Historic Bridge Foundation
Rhys Martin, Oklahoma Route 66 Association	Kristina Wyckoff, Oklahoma SHPO
Lynda Ozan, Oklahoma SHPO	Chantry Banks, Preservation Oklahoma, Inc.
Karen Orton, Federal Highway Administration	Ralph Nguyen, Federal Highway Administration
Anne Haaker, Route 66, The Road Ahead	Kaisa Barthuli, NPS Route 66 Preservation Program
Marilyn Emde, Oklahoma Route 66 Association	

1. Introductions

Scott Sundermeyer, ODOT Cultural Resources Program

- a. The meeting opened with introductions for each attendee listed above.
- b. Participants were advised that the presentation would be recorded.

2. Presentation

Scott Sundermeyer, ODOT Cultural Resources Program

Gregg Hostetler, CONSOR Engineer

- a. Project background and Section 106 Consultation history
 - i. The bridge is the only resource in the project area that is NRHP-eligible.
 - ii. The roadbed is not eligible for listing in the NRHP.
- b. Bridge Details
 - i. The bridge is eligible under Criterion C for its engineering and design.
 - ii. Route 66 original alignment (just north and west)
 1. If anyone has photos of the original bridge ODOT would appreciate them.
- c. Purpose and Need
 - i. To provide a bridge crossing that is structurally and functionally sufficient for the intended use of the structure.
 - ii. The bridge is in poor condition and fracture critical.
- d. Discussion of current bridge conditions and maintenance and repairs conducted on the bridge.
- e. Alternatives considered under Section 4(f) preliminary evaluation:
 - i. Do Nothing
 - ii. Build on New Location
 1. Retain existing bridge as monument or non-motorized pedestrian or bicycle facility
 - iii. Rehabilitation without affecting historic integrity
 - iv. Additional Alternatives reviewed in Design Analysis
 1. Rehabilitate existing; widen to provide 38-ft roadway

- 2. Replacement on existing alignment
- f. Summary of the Alternatives evaluated in the Design Analysis report, the potential effect on the historic bridge and the recently updated costs
 - i. Alternative 1
 - 1. Do Nothing – Requires maintenance. Doesn't meet purpose and need.
 - ii. Alternative 2(a)
 - 1. Includes widening. Bridge would remain fracture critical.
 - iii. Alternative 2 (b)
 - 1. Rehab, maintain current roadway width (more in-line with Section 4(f).) Bridge remains fracture critical.
 - iv. Alternative 3
 - 1. Six offset alignments were evaluated in March 2011.
 - 2. Effect determination will require consultation with SHPO.
 - v. Alternative 4
 - 1. Replace existing bridge on existing alignment - adverse effect and mitigation
- g. "Next Steps" and upcoming milestones
 - i. Consulting Parties to review and comment on Design Analysis Document
 - ii. Public Meeting anticipated Winter 2021 (in person or virtual)
 - iii. Preferred Alternative anticipated Spring 2022
- h. Comments can be provided on the project website, via email or written letters
 - i. <http://www.odotculturalresources.info/bird-creek-bridge.html>

3. Discussion/Questions

Scott Sundermeyer, ODOT Cultural Resources Program

- a. Ms. Henderson wanted to confirm the design analysis matrix is available online
 - i. Yes, the matrix is on the project website
- b. Mr. Martin inquired whether there are major differences between what we can do with the NB bridge and the options that were considered for the SB bridge.
 - i. All alternatives considered in 2011 for the 1936 bridge are being looked at. Additionally, locations for avoidance, etc., will be reviewed. However, there are challenges because this area is constricted.
 - ii. The intent of this process is to look at ideas resulting from this engagement.
- c. Ms. Henderson inquired how many K-trusses would remain in Oklahoma if this bridge were removed.
 - i. Not many K-trusses would remain, and the K-trusses that are still in Oklahoma are largely on-system.
 - ii. ODOT is currently looking into a large-scale context study to look at the remaining K-truss bridges and a consultant has been identified to prepare that documentation. There may be approximately 15 K-truss bridges remaining. Ms. Henderson noted that nationwide there are very few remaining.
- d. Ms. Barthuli inquired whether other historical factors and values are being considered with regard to the Route 66 roadway through Oklahoma. Such as, how many historic bridges remain along Route 66 in Oklahoma and which ones will be prioritized for preservation/retention?
 - i. ODOT is conducting an updated study and a multi-property document for all of the bridges along Route 66, which may identify additional historic bridges along the roadway.
 - 1. Ms. Barthuli noted that such a study could lead to prioritization of particular bridges and additional investment in those bridges that would help to preserve the bridges.
 - ii. A department-wide plan would have to be discussed with ODOT senior leadership, but ODOT would be open to having the discussion.
 - 1. Ms. Barthuli further noted the tremendous benefit and impact associated with Route 66. She strongly encouraged special prioritized treatment and planning of Route 66 bridges and remaining pristine roadbed.

- e. Mr. Martin questioned whether there has been any discussion/consideration of utilizing a design similar to what has been done with pony bridges, with the trusses removed and used as an aesthetic feature but not load bearing.
 - i. Ms. Henderson noted there was a successful widening of a through-truss in Vermont, she said she will try to share information on that project

4. Additional Items

Scott Sundermeyer, ODOT Cultural Resources Program

- a. Would any party prefer a hard copy of the Design Analysis document?
 - i. SHPO will be provided with a hard copy. A digital version is acceptable to all others.
- b. ODOT will provide notification of the public meeting.
- c. The recording and presentation will be posted on the website.



Consulting Party Meeting

SH-66 NB over Bird Creek
Rogers County, Oklahoma
JP 20899(09)

September 20, 2021

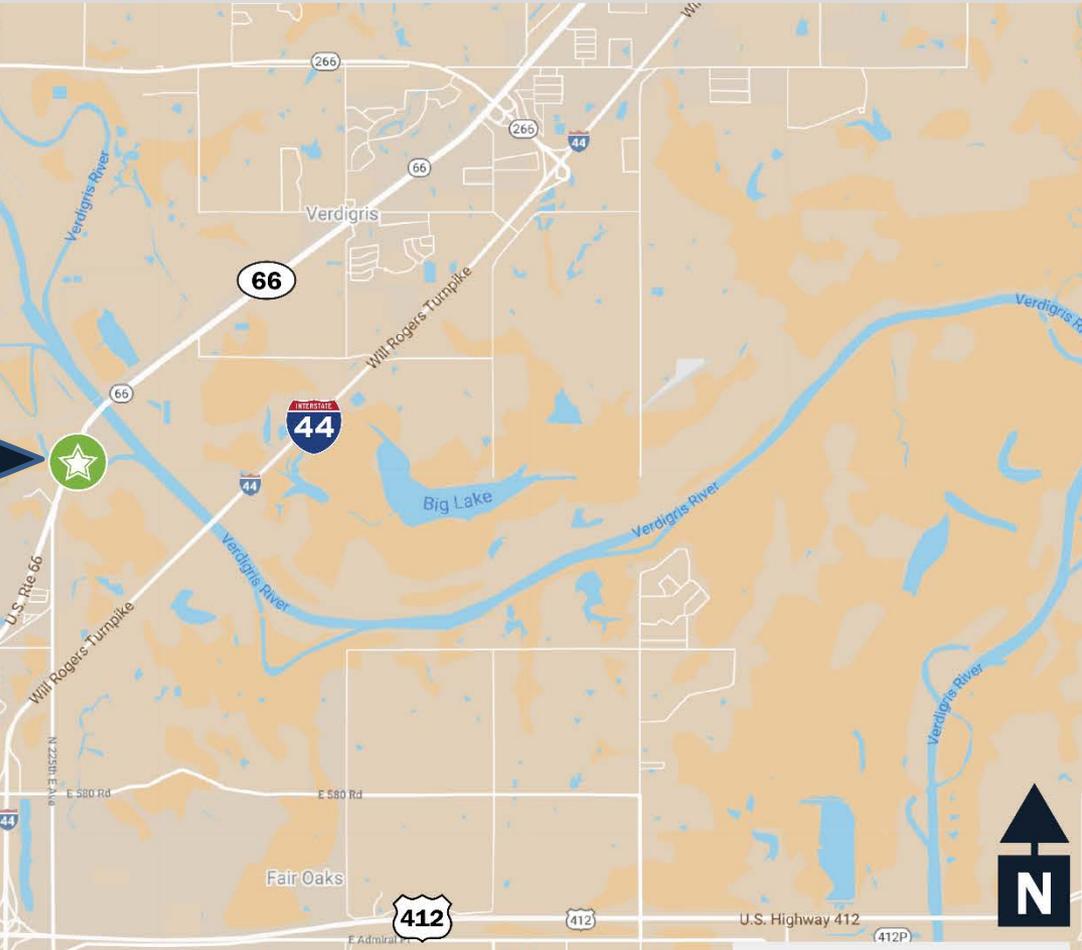
Section 106 Consultation History

- **November 11, 2019 - Initiated Consultation with SHPO and Consulting Parties**
- **May 1 and May 12, 2020 – Provided cultural resources report of investigations to SHPO, State Archaeologist, and Consulting Parties**
 - **No other historic properties identified**
 - **Concurrence from SHPO that roadbed is not eligible for listing on the NRHP**
 - **Request to describe previous work conducted on the bridge**
 - **Provided list of alternatives being considered**

SH-66 over Bird Creek: Bridge Details

- **Six-span mixed truss bridge:**
 - *Three Camelback Trusses, Three K-Trusses*
- **Eligible for listing on the National Register of Historic Places (NRHP), under criterion C**
- **Constructed in 1956**
- **Classified as structurally deficient (poor condition)**
- **Trusses and floorbeams are classified as fracture critical**

SH-66 over Bird Creek



 Project Location

Purpose and Need of the Project:

Purpose:

- The project purpose defines the problem (need) to be solved.
 - Provide a bridge crossing that is structurally and functionally sufficient for the intended use of the structure.

Need:

- The project need describes the transportation deficiency. It is the foundation of the entire decision-making process. The need provides information to support the purpose
 - The existing bridge is in poor condition and is fracture critical

Bridge Condition

- Bridge Inspection Reports

- Routine and Fracture Critical
- Other Special

- Condition Ratings (December 2020)

- Deck = 5 (Fair)
- Superstructure = 4 (Poor)
- Substructure = 6 (Satisfactory)
- Sufficiency Rating= 49.4

- Other concerns

- Existing 30 ft. Roadway (AASHTO Standard: 36 ft. Minimum Roadway)
- Non-standard clearance of 15'-6" (AASHTO Standard: 16')
- Not currently load-posted, but should be posted for 32 tons
- Daily Traffic 7900



Maintenance and other repairs

- 20926(04) – Bridge Paint project – consulted Spring 2013
- 20926(05) – Bridge Joint/Seal Repair – consulted February 2015
- Repair Bridge Deck – April 2019
- Clean/Rehabilitate/Replace Bridge Joints - September 2019
- Repair Bridge Deck – January 2020
- Rehabilitate Floor System – March 2020
- Repair Bridge Deck – June 2020
- Repair Bridge Deck - November 2020
- Continuous maintenance (deck washing)



Alternatives Considered - Section 4(f)

- Alternatives that do not ‘use’ bridge (result in no adverse effect)
 - Do Nothing
 - Build on New Location
 - Retain existing bridge as non-functional “monument”
 - Retain existing bridge as non-motorized pedestrian or bicycle facility
 - Rehabilitation without Affecting Historic Integrity of Bridge
- Additional Alternatives reviewed in Design Analysis
 - Rehabilitate existing bridge; widen to provide 38-foot roadway width
 - Replacement of Bridge on Existing Alignment

Design Analysis Report Summary

Alternative 1 – Do Nothing

- Minor maintenance only; no major rehabilitation
 - Periodic closures likely
 - Regular maintenance
 - Costly inspections



Design Analysis Report Summary

Alternative 2: Rehabilitation Without Affecting Historic Integrity of the Bridge

- Alternative 2(a): Rehabilitation existing bridge; widen to provide 38-ft roadway
 - Replacement of deck, approach slabs, stringers, floor beams, and lateral bracing
 - Strengthening of various truss members
 - Remove and reset trusses and bearings
 - New bridge rails
 - Painting of all structural steel
 - Augmentation of both abutments and all five piers
 - Minor substructure repairs

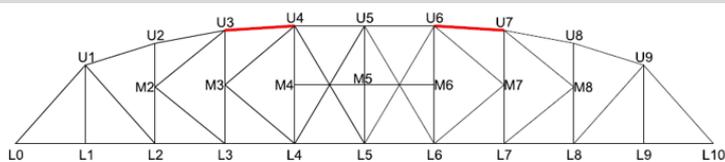


Figure 35: Overstressed Truss Members (Span 3) – Rehabilitation with Widening

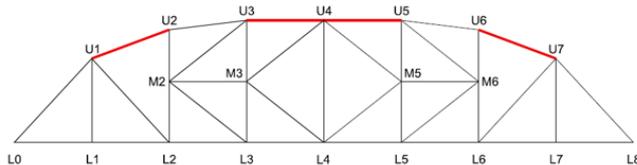


Figure 36: Overstressed Truss Members (Span 4) – Rehabilitation with Widening

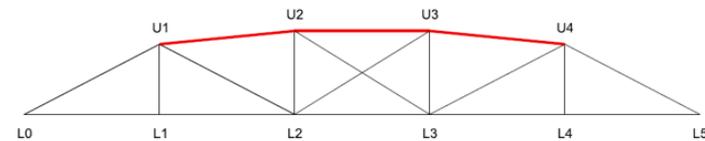


Figure 33: Overstressed Truss Members (Spans 1, 5, and 6) – Rehabilitation with Widening

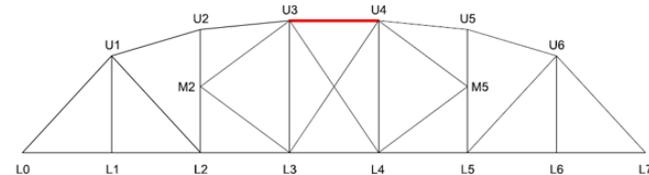
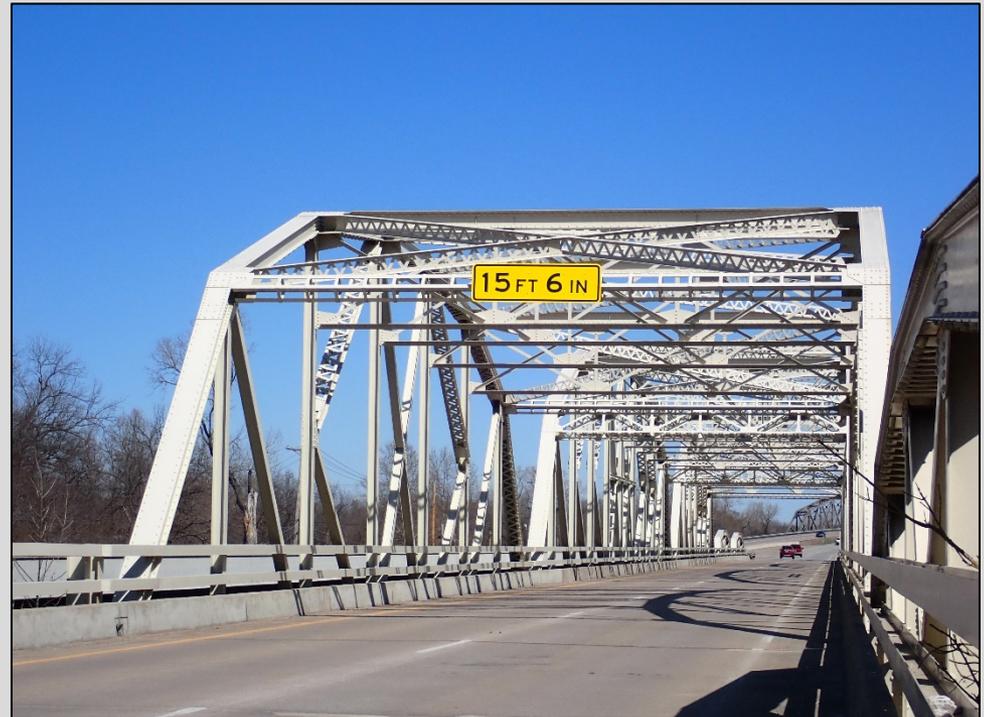


Figure 34: Overstressed Truss Members (Span 2) – Rehabilitation with Widening

Design Analysis Report Summary

Alternative 2: Rehabilitation Without Affecting Historic Integrity of the Bridge

- Alternative 2(b): Rehabilitation of existing bridge; maintain current 30-ft roadway width
 - Replacement of deck, approach slabs, stringers, and floor beams
 - Remove and reset trusses and bearings
 - New bridge rails
 - Painting of all structural steel
 - Minor substructure repairs



Design Analysis Report Summary

Alternative 3 - Build on New Location

- Existing bridge remains in place as:
 - Non-functional monument
 - Non-vehicular pedestrian and/or bicycle facility
- Six offset alignments were evaluated in March 2011.



Design Analysis Report Summary

Alternative 4: Replacement of Bridge on Existing Alignment

- Existing bridge removed
- Result in adverse effect and would require mitigation



Design Analysis Report Summary

Summary of Findings

Category	Avoidance Alternatives				#4: New Bridge on Existing Location
	#1: Do Nothing	#2: Rehabilitation		#3: Build on New Location, Leave in Place as Monument	
		#2(a): Widen	#2(b): No Widening		
Maintenance and Inspection	<ul style="list-style-type: none"> Increased inspection frequency Increased frequency of repairs to address section loss in steel, particularly stringers and floorbeams Increased frequency of maintenance required, including spot painting 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Structural repairs should not be required if regular program of cleaning the trusses and spot painting areas of corrosion is initiated 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Structural repairs should not be required if regular program of cleaning the trusses and spot painting areas of corrosion is initiated 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-25 years, after which spot painting will be required Significantly reduced inspection effort 	<ul style="list-style-type: none"> Minimal maintenance required for first 20-30 years Inspection effort significantly reduced because new bridge will no longer be fracture critical
Geometric Adequacy	<ul style="list-style-type: none"> Bridge remains Functionally Obsolete 	<ul style="list-style-type: none"> Bridge no longer Functionally Obsolete Provides 38 feet clear roadway width 	<ul style="list-style-type: none"> Bridge remains Functionally Obsolete 	<ul style="list-style-type: none"> Bridge no longer Functionally Obsolete, as it is no longer open to vehicular traffic 	<ul style="list-style-type: none"> Bridge meets current AASHTO and ODOT geometric standards Functionally Obsolete bridge removed from service
Structural Adequacy	<ul style="list-style-type: none"> Requires load posting Remains Structurally Deficient Remains Fracture Critical 	<ul style="list-style-type: none"> No load posting required No longer Structurally Deficient Remains Fracture Critical 	<ul style="list-style-type: none"> No load posting required No longer Structurally Deficient Remains Fracture Critical 	<ul style="list-style-type: none"> Monument use requires fencing or other means to keep public off bridge, while allowing access for maintenance vehicles No longer considered Structurally Deficient as it is no longer carrying vehicles 	<ul style="list-style-type: none"> Load posted, Structurally Deficient, and Fracture Critical bridge removed from service

Design Analysis Report Summary

Category	Avoidance Alternatives				#4: New Bridge on Existing Location
	#1: Do Nothing	#2: Rehabilitation		#3: Build on New Location	
		#2(a): Widen	#2(b): No Widening		
Environmental	<ul style="list-style-type: none"> Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel 	<ul style="list-style-type: none"> Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel Jurisdictional Waters and Wetlands 	<ul style="list-style-type: none"> Marginal habitat for Northern Long-eared Bat, Least Tern, Piping Plover, Red Knot, Whooping Crane, American Burying Beetle, Neosho Mucket, and Rabbitsfoot Mussel Jurisdictional Waters and Wetlands
Permits	<ul style="list-style-type: none"> None Anticipated 	<ul style="list-style-type: none"> US Army Corps of Engineers – Nationwide 14 Flood Plain Permit (County) DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> US Army Corps of Engineers – Nationwide 14 Flood Plain Permit (County) DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> US Army Corps of Engineers – Nationwide 14 Flood Plain Permit (County) DEQ OK R10 (Construction Stormwater Permit) 	<ul style="list-style-type: none"> US Army Corps of Engineers – Nationwide 14 Flood Plain Permit (County) DEQ OK R10 (Construction Stormwater Permit)
Adverse Effects on Historic Bridge	<ul style="list-style-type: none"> None Anticipated 	<ul style="list-style-type: none"> Rebuilt bracing system Potentially adverse effect due to substructure modification Modifications to floor system and truss members not anticipated to have adverse effect 	<ul style="list-style-type: none"> Modifications to floor system and truss members not anticipated to have adverse effect Bridge retains appearance and function 	<ul style="list-style-type: none"> Effect determination will require consultation with SHPO – expected that work will not cause an adverse effect to the character defining features of the bridge, but change of use will likely be an adverse effect due to bridge being on historic route 	<ul style="list-style-type: none"> Truss spans could be placed on display near the surrounding area similar to the previous westbound bridge
Construction Cost (Bridge Only)	\$ 616 thousand	\$ 9.1 million	\$ 7.1 million	\$ 10 thousand for existing bridge; \$6.9 million for new bridge	\$ 7.3 million
20-Year Maintenance & Inspection Cost (2020 Dollars)	\$ 540 thousand	\$ 240 thousand	\$ 215 thousand	\$ 34 thousand	\$ 30 thousand

Next Steps

- **Submit Design Analysis document to consulting parties (Summer 2021)**
- **Receive comment (Fall 2021)**
- **Additional Public Involvement**
 - **Public Meeting - Open House Format (Winter 2021)**
- **Selection of Preferred Alternative (Spring 2022)**

Project Website

- Check website for access to project information and reports as they are available
- Provide comments via website
- <http://www.odotculturalresources.info/bird-creek-bridge.html>

We value your input!

From: [Scott Sundermeyer](#)
To: [Kelly Saladis](#); [Erin Faulkner](#); [ghostetler](#); [Joe Brutsche](#); [Jennifer Bullard](#); [Jenny Droscher](#); [Justin Hernandez](#); [Katie Brown](#); [Karen Orton - FHWA](#); [Matt Casillas](#); [Nguyen, Ralph \(FHWA\)](#); [Rick Mitchell](#); [Randle White](#); [Sara Downard](#); [Siv Sundaram](#); [Victoria Raines](#); [kitty@historicbridgefoundation.com](#); [rt66theroadahead@gmail.com](#); [Margaret_Frisbie@nps.gov](#); [anneehaaker@gmail.com](#); [jblish@cityofcatoosa.org](#); [Kris Marek](#); [rhys.martin@oklahomaroute66.com](#); [Cari Foster](#); [director@preservationok.org](#); [kaisa_barthuli@nps.gov](#); [Lynda Ozan](#); [info@oklahomaroute66.com](#)
Subject: RE: Virtual Section 106 Consulting Party Meeting - Rogers County 20899(09) SH-66 Bird Creek Bridge
Date: Thursday, September 9, 2021 4:50:00 PM

Good evening –

Thank you for your continued participation in this undertaking. The project website is complete (<http://www.odotculturalresources.info/bird-creek-bridge.html>) and the Design Analysis engineering report has been posted, along with all prior Section 106 consultation. Please feel free to review this material in advance of our meeting on September 20.

Best-
Scott

Scott A. Sundermeyer
Cultural and Natural Resources Program Director
Desk: 405-325-7201
Cell: 405-388-2201
ssundermeyer@odot.org

-----Original Appointment-----

From: Kelly Saladis <ksaladis@cpyi.com>

Sent: Wednesday, September 8, 2021 8:54 PM

To: Kelly Saladis; Erin Faulkner; ghostetler; Joe Brutsche; Jennifer Bullard; Jenny Droscher; Justin Hernandez; Katie Brown; Karen Orton - FHWA; Matt Casillas; Nguyen, Ralph (FHWA); Rick Mitchell; Randle White; Sara Downard; Siv Sundaram; Scott Sundermeyer; Victoria Raines; kitty@historicbridgefoundation.com; rt66theroadahead@gmail.com; Margaret_Frisbie@nps.gov; anneehaaker@gmail.com; jblish@cityofcatoosa.org; Kris Marek; rhys.martin@oklahomaroute66.com; Cari Foster; director@preservationok.org; kaisa_barthuli@nps.gov; Lynda Ozan; info@oklahomaroute66.com

Subject: Virtual Section 106 Consulting Party Meeting - Rogers County 20899(09) SH-66 Bird Creek Bridge

When: Monday, September 20, 2021 1:00 PM-2:00 PM (UTC-06:00) Central Time (US & Canada).

Where: Microsoft Teams Meeting

ODOT would like to invite you to a virtual Section 106 consulting party meeting to discuss the Purpose and Need of the project, present the alternatives analysis, and obtain comments from you. At this time, we are still relatively early in the process and your input is valuable.

The project website is being developed and will be complete this week (<http://www.odotculturalresources.info/bird-creek-bridge.html>). All information will be posted on the website.

Microsoft Teams meeting

Join on your computer or mobile app

[Click here to join the meeting](#)

Or call in (audio only)

[+1 469-607-1888,,569905350#](#) United States, Dallas

[\(844\) 428-2394,,569905350#](#) United States (Toll-free)

Phone Conference ID: 569 905 350#

[Find a local number](#) | [Reset PIN](#)



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From: [Scott Sundermeyer](#)
To: "Kitty Henderson"; [The Road Ahead](#); [Barthuli, Kaisa M](#); [Cayla Lewis](#); [Lynda Ozan](#); [Rhys Martin](#); [Oklahoma Route 66 Association](#); [Frisbie, Margaret X](#); annehaaker@gmail.com; jblish@cityofcatoosa.org; [Kris Marek](#)
Cc: [Kelly Saladis](#); [Victoria Raines](#)
Subject: Rogers County 20899(09) SH-66 Bird Creek Bridge Section 106 consulting party meeting
Date: Tuesday, September 7, 2021 3:20:00 PM

Good afternoon –

We consulted with you in November 2019 and May 2020 to present ODOT's desire to address deficiencies in the Bird Creek bridge near Catoosa, Oklahoma. This truss bridge was constructed in 1956 and consists of three K-truss spans with three camelback pony trusses. We provided a cultural resources report of investigations and an outline of the alternatives that ODOT was considering to improve this crossing.

ODOT would like to invite you to a virtual Section 106 consulting party meeting on **September 20 at 1:00 p.m.** Central time to discuss the Purpose and Need of the project, present the alternatives analysis, and obtain comment from you. At this time, we are still relatively early in the process and your input is valuable.

Our environmental consultant will be sending a meeting invitation to you in the next couple of days, which will contain the link to the virtual meeting. I will be presenting the information along with an engineer who is familiar with historic bridges.

The project website is being developed and will be complete this week (<http://www.odotculturalresources.info/bird-creek-bridge.html>). All information will be posted on the website.

Thank you very much, and please do not hesitate in contacting me with any questions.

Scott

Scott A. Sundermeyer
Cultural and Natural Resources Program Director
Oklahoma Department of Transportation
3200 Marshall, Room 110
Norman, OK 73019
Desk: 405-325-7201
Cell: 405-388-2201
ssundermeyer@odot.org



Oklahoma Historical Society
State Historic Preservation Office

Founded May 27, 1893

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

May 21, 2020

Mr. Scott Sundermeyer, Director
ODOT Cultural Resources Program
111 East Chesapeake, Rm. 102, OU
Norman, OK 73019

RE: File #0562-20; Bird Creek Bridge on SH-66 Proposed Improvements Project, #JP020899(09),
Rogers County, Oklahoma

Dear Mr. Sundermeyer:

On May 4, 2020, we received for review the additional information for the referenced undertaking submitted with your letter dated May 1, 2020. We concur with your opinion that the Route 66 roadbed segment over Bird Creek (northbound) is not eligible for listing on the National Register of Historic Places (NRHP). In addition, we defer comment on project effect to the archaeological site, 34RO15, to the State Archaeologist with the Oklahoma Archeological Survey.

That being said, our statement in our December 11, 2019 letter regarding the proposed project for the historic eastbound bridge over Bird Creek (ODOT Structure #6602-0368EX/NBI-13688), which was previously determined eligible for listing on the NRHP in 2007, still stands. Because this bridge's westbound twin was demolished in 2012, the current project for the eastbound bridge could be considered a cumulative adverse effect.

Finally, as you continue to consider alternatives, please be sure to describe the previous work conducted on the bridge, including the work described in SHPO File #0852-15 (ODOT Project #JP-20926[05]), and explain why the repairs have not been successful.

Thank you for the opportunity to review and comment on this project. If you have any questions, please call Catharine M. Wood, Historical Archaeologist, at 405/521-6381. Please reference the above underlined file number when responding. Thank you.

Sincerely,

Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm



Oklahoma Archeological Survey

THE UNIVERSITY OF OKLAHOMA

June 10, 2020

Scott Sundermeyer, Director
ODOT Cultural Resources Program
111 E. Chesapeake, Rm 102
University of Oklahoma
Norman, OK 73019-5111

Re: OAS FY20-2029: FHWA Project JP 20899(09): Proposed Improvements to the Historic Eastbound Bridge Carrying SH-66 over Bird Creek
Legal Location: Sections 16, 20-21, T20N, R15E
Rogers County

Dear Mr. Sundermeyer,

This agency received the submitted ODOT cultural resources survey report of investigations regarding the above-referenced undertaking for review and comment. From the information provided, I understand that ODOT-CRP staff surveyed the 50.88-acre study area on November 14, 2019. No archaeological sites or historic buildings were documented in the study area. The bridge has been determined eligible for inclusion in the National Register of Historic Places (NRHP); however, ODOT recommends that the segment of Route 66 within the project area is not NRHP-eligible. From the information provided, I understand that ODOT is only seeking comment on the determination that there are no other historic properties in the preliminary Area of Potential Effects (APE) besides the bridge.

I concur with the findings and recommendations as they pertain to the prehistoric resources and defer further comment on the determination of Historic Properties present in the APE and overall project effects to the State Historic Preservation Office.

This review has been conducted in cooperation with the State Historic Preservation Office, Oklahoma Historical Society. You must also have a letter from that office to document your consultation pursuant to Section 106 of the National Historic Preservation Act.

Sincerely,

Kary L. Stackelbeck

Kary L. Stackelbeck, Ph.D.
State Archaeologist

cc: SHPO

May 1, 2020

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Dear Ms. Ozan:

Re: SHPO File no. 0562-20; Rogers County FHWA Project JP 20899(09): Proposed improvements to the historic eastbound bridge carrying SH-66 over Bird Creek; submittal for comment under Section 106 of the National Historic Preservation Act.

Thank you for your comments of December 11, 2019. In your response you concurred with our Area of Potential Effect (APE) we defined; you indicated the proposed project for the eastbound bridge may be considered a cumulative adverse effect, and you recommended ODOT take into consideration the larger, indirect impact of the project on the archaeological site (34RO15) and on the surrounding bridges identified on the map. We appreciate comments like these as we move forward with the project.

Please find the attached cultural resources survey report for the referenced project prepared by the ODOT Cultural Resources Program. ODOT has conducted a cultural resources study within existing right-of-way at the bridge location. The preliminary area of potential effect (APE) as defined by 36 CFR 800.16(d) is the project study area, which is confined to the existing right-of-way and is described in the report.

In your comments of December 11, 2019, you mentioned potential impacts to an archaeological site, 34RO15, mapped in the project vicinity. As we discuss in the attached cultural resources report, the 34RO15 site form was based on a description provided by a collector, no archaeological materials have been observed at this location by prior investigations (1976, 1982), and no archaeological materials or buried soil horizons were observed in the project APE or immediate vicinity during the current archaeological investigations. Because no evidence of archaeological site 34RO15 was observed or identified during our investigations, the proposed project is not considered to have any direct or indirect impact on this archaeological site. Additionally, an avoidance memo for off-site activities for the mapped site location will be added to the project plans.

The roadway portion of the APE was subjected to xx projects since 1925. The full history is as follows:

- 1925
Project ID: FAP-183AC & SAPF-183-B
Grade & Drain, Roadway 30' wide
- 1927
Project ID: FAP-183A & SAPF-183A(Ext)
PC Concrete 9" Thick, Roadway 18' Wide
- 1936
Project ID: NRH-183AB
PC Concrete 10" Thick, Roadway Divided 44' Wide Total Paved Area
- 1958
Project ID: F-183(7)
Asphalt 5" Thick, Roadway Divided 48' Wide Total Paved Area

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- 1970 (~3.49 miles to end)
Project ID: SAP-66(15)
Full Depth Reconstruct PC Concrete (8" thick) over Hot Sand Asphalt (4" thick),
Roadway Divided 48' Wide Total Paved Area
- 1979
Project ID: MC-66(77)pf-1 (Not sure if that is an ' f ' or not; old record is a little hard to read)
Asphalt Overlay 1.5" Thick, Roadway Divided 48' Wide Total Paved Area
- 1988
Project ID: MC-66(261)
Asphalt Overlay w/Fabric Reinforcement 1.5" Thick, Roadway Divided 48' Wide Total
Paved Area
- 2004 (Beginning to ~3.50 miles)
STATE FORCES
Asphalt Overlay 1" Thick, Roadway Divided 48' Wide Total Paved Area
- 2018
STATE FORCES
Asphalt Overlay Unknown (probably 1" or 1.5") Thick, Roadway Divided 48' Wide
Total Paved Area

The segment of Route 66 within the project is not one of the segments designated as being significant in the 2002 Oklahoma Route 66 Historic Resources Survey, 1926-1970. According to the 2002 report, historic Route 66 followed the westbound lane and not the eastbound lanes; the historic route digressed from the westbound lanes of the four-lane near the navigation system. In 1936, this alignment was bypassed from that point westward to a connection with the 1926 route just beyond the 1936 river bridge. In 1957, the highway in this area was four-laned adding two or four lanes where needed. The approaches to the navigation system were later elevated when its buildings were built in the 1960s.

Based on this information, it is our opinion that this segment of roadway retains its association with Route 66. However, it does not retain integrity of design, materials, workmanship, and setting, and these aspects of integrity are critical for the roadbed to merit consideration for inclusion in the NRHP. Pursuant to 36 CFR 60.4, it is our opinion this segment of Route 66 lacks historic integrity and is not eligible for inclusion in the NRHP. Please find attached the Historic Preservation Resource Identification form for this segment of Route 66.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688)]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

ODOT is currently considering a number of alternatives to meet the purpose and need of this project, as outlined in the National Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges. These alternatives include:

1. Do nothing ["no build" or no action option]
2. Build a new structure at a different location and leave the existing historic bridge in place to function as either a pedestrian bridge or a monument.

3. Rehabilitation without affecting the historic integrity of the bridge.

In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. At this time, we respectfully request your comment on this determination. In accordance with regulations implementing Section 106 of the NHPA, as amended, and Section 4(f) of the USDOT Act, ODOT will pursue future consultation on the project's effect to historic properties.

We would like to invite you to a meeting of FHWA, ODOT, and consulting parties to discuss these and very soon the end of March or beginning of April, and we will be reaching out to you for your availability. A PowerPoint presentation will be provided in advance of the meeting.

If you have any questions regarding this project, please contact me at 325-7201.

Sincerely,



Scott Sundermeyer
Director, ODOT Cultural Resources Program

cc: State Archaeologist

May 12, 2020

Dear Stakeholder:

Re: Rogers County Federal Highways Administration (FHWA) Project JP 20899(09): Proposed improvements to the historic (eastern) northbound bridge carrying SH-66 over Bird Creek.

The Oklahoma Department of Transportation (ODOT) is proposing improvements to the historic (eastern) northbound bridge carrying US-66 over Bird Creek in Catoosa, Oklahoma. The existing bridge is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957. This bridge was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the National Register of Historic Places (NRHP) under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. This bridge is a historic property and will require additional consultation with the State Historic Preservation Office (SHPO) and other consulting parties.

ODOT is currently considering alternatives to meet the purpose and need of this project. These include:

1. Do nothing ["no build" or no action option]
2. Build a new structure at a different location and leave the existing historic bridge in place to function as either a pedestrian bridge or a monument.
3. Rehabilitation without affecting the historic integrity of the bridge.

In consideration of these alternatives, please find the attached cultural resources survey report for these investigations. The segment of Route 66 within the project study area was assessed as not eligible for inclusion in the NRHP. As described above, the project bridge is eligible for inclusion in the NRHP, but no additional historic properties were identified as a result of the cultural resources investigation.

At this time, we respectfully request your comment on our determination that no additional historic properties are within the project Area of Potential Effect (APE). In accordance with regulations implementing Section 106 of the NHPA, as amended, and Section 4(f) of the USDOT Act, ODOT will pursue future consultation on the project's effect to historic properties.

If you have any questions regarding this project, please contact me at 325-7201.

Sincerely,



Scott Sundermeyer
Director, ODOT Cultural Resources Program

cc: Catoosa Historic Society Museum
City of Catoosa
Historic Bridge Foundation
National Park Service Route 66 Corridor Preservation Program
Oklahoma Bridge and Highway Group
Oklahoma Route 66 Association, Inc.
Oklahoma Tourism and Recreation Department
Oklahoma Public Archeology Network
Preservation Oklahoma
Route 66 Road Ahead Partnership

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OKLAHOMA DEPARTMENT OF TRANSPORTATION

CULTURAL RESOURCES SURVEY REPORT

Rogers 20899(09): SH-66 northbound bridge over Bird Creek in Catoosa

Prepared by ODOT Cultural Resources Program
Kirsten Tharalson, archaeologist
Kristina Wyckoff, archaeologist
Cari Foster, architectural historian

April 27, 2020

Lead Federal Agency: Federal Highways Administration (FHWA)



County:	Rogers
J/P#:	20899(09)
Surveyed by:	Kirsten Tharalson and Kristina Wyckoff
Survey Date:	November 14, 2019

MANAGEMENT SUMMARY:

ODOT Cultural Resources Program (CRP) conducted a Phase I cultural resources investigation for proposed improvements to the existing (eastern) bridge carrying northbound SH-66 over Bird Creek. The project study area comprises a corridor of SH-66 and is confined to the existing right-of-way; the study area encompasses approximately 50.88 acres. A prior (2007) ODOT CRP investigation for the replacement of the (western) bridge carrying southbound SH-66 over Bird Creek investigated the western portion of the right-of-way; the eastern half of the right-of-way, comprising 25.29 acres, was subjected to pedestrian archaeological survey with shovel tests excavated at 30 meter intervals as part of the current investigation.

No archaeological sites or buildings were recorded or documented in the project study area. The existing segment of SH-66 is not eligible for inclusion in the National Register of Historic Places.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C (Spans of Time Reevaluation 2007: 2) but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the project study area and has identified no additional historic properties. This report was submitted to the Oklahoma SHPO, the State Archaeologist, the City of Catoosa, the Catoosa Historic Society Museum, the Historic Bridge Foundation, the National Park Service Route 66 Corridor Preservation Program, the Oklahoma Bridge and Highway Group, the Oklahoma Route 66 Association, Inc., The Oklahoma Tourism and Recreation Department, the Oklahoma Public Archaeology Network, Preservation Oklahoma, and the Route 66 Road Ahead Partnership, and ODOT has requested comment on our determination that no additional historic properties are present in the study area. Additional consultation with SHPO and other consulting parties will be necessary to avoid, minimize, and mitigate effects to the National Register eligible SH-66 northbound bridge over Bird Creek.

1. PROJECT DESCRIPTION:

This report documents a cultural resources investigation for proposed improvements to the extant bridge carrying SH-66 northbound over Bird Creek in Catoosa. The existing SH-66 facility is a divided highway comprised of two 11-foot northbound lanes and two 11-foot southbound lanes, each with three-foot inside and outside concrete shoulders.

The project study area comprises a 7,387-foot long corridor of SH-66 bisected in the center by the existing bridge. The study area extends from right-of-way to right-of-way, 150-feet east and west of the existing SH-66 center throughout the study area corridor. In total, the current study area encompasses approximately 50.88 acres (see Figure 1).

The current project study area overlaps a prior ODOT cultural resources investigation (Rogers 20889(04)), for replacement of the western (southbound) bridge carrying SH-66 over Bird Creek, which covered the western half (25.29 acres) of the current study area.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C (Spans of Time Reevaluation 2007: 2) but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

The existing (western) bridge carrying SH-66 southbound over Bird Creek (ODOT Structure # 6602 0368 WX / NBI 29984) is a steel stringer/multi-beam/girder constructed in 2012. The bridge is less than 45 years of age and was therefore not documented during the current investigation.

The existing bridge carrying SH-66 northbound over the Verdigris River (ODOT Structure # 6602 0417 EX / NBI 17603) is a steel girder and floorbeam structure with concrete deck and supports constructed in 1969. The bridge is of the type discussed in the Advisory Council on Historic Preservation (ACHP) Program Comment for post-1945 concrete and steel bridges and required no additional documentation.

The existing bridge carrying SH-66 southbound over the Verdigris River (ODOT Structure # 6602 0417 WX / NBI 17604) is a steel girder and floorbeam structure with concrete deck and supports constructed in 1969. The bridge is of the type discussed in the ACHP Program Comment for post-1945 concrete and steel bridges and required no additional documentation.

The segment of Route 66 within the project was originally constructed as a 30-foot wide roadway in 1925. The original roadway followed the alignment of the existing westbound lanes and has undergone considerable change including a full depth reconstruction in 1970. It is our opinion the segment of Route 66 within the current ODOT project study area retains its association with Route 66, however it does not retain integrity of design, materials, workmanship, or setting. It is our opinion this segment of Route 66 lacks historic integrity and is not eligible for inclusion in the NRHP.

Legal Location:	T20N R15E Sections 16, 20-21
U.S.G.S. Quadrangle:	Catoosa [1963 (1980)]

2. ENVIRONMENTAL SETTING:

Geomorphic/Physiographic Region:

The project study area is mapped in the Claymore Cuesta Plains geomorphic province where resistant Pennsylvanian sandstones and limestones dip gently westward to form cuestas between broad shale plains.

Geology and Soils:

The project study area is mapped across Pennsylvanian Senora Formation and Quaternary Alluvium deposits.

As mapped, soils and sediments in the study area belong to the Hector, Barge, Endsaw, Osage, and Verdigris soil series. Soils mapped in the Hector series are described as having a very shallow dark brown gravelly fine sandy loam A horizon [0-5 centimeters below surface (cmbs)], over a shallow brown fine sandy loam A horizon (5-15 cmbs), over a moderately shallow strong brown fine sandy loam B horizon (15-38 cmbs), overlying a sandstone bedrock R horizon (38-76 cmbs). Soils mapped in the Barge series are described as having a shallow brown silty clay loam Ap horizon (0-18 cmbs) overlying a dark yellowish-brown silty clay loam C horizon (18-183 cmbs). Soils mapped in the Endsaw series are described as having a shallow dark grayish brown cobbly fine sandy loam A horizon (0-10 cmbs), over a moderately shallow light yellowish brown cobbly fine sandy loam E horizon (10-36 cmbs), over a series of yellowish red clay B horizons (36-86 cmbs), over a mottled yellowish brown and gray clay BC horizon (86-107 cmbs), overlying an olive gray and gray soft shale Cr horizon (107-152 cmbs). Soils mapped in the Osage series are described as having a shallow very dark gray rubbed clay Ap horizon (0-10 cmbs), over a shallow very dark gray rubbed silty clay Ap horizon (10-20 cmbs), over a shallow very dark gray rubbed clay A horizon (20-33 cmbs), over a series of very dark gray and dark gray clay B horizons (33-152 cmbs), over a dark gray silty clay B horizon (152-200 cmbs).

Soils mapped in the Verdigris series are typically described as having a shallow very dark brown Ap horizon (0-18 cmbs), over a thick very dark brown silt loam A horizon (18-71 cmbs), over a thick dark brown silt loam AC horizon (71-117 cmbs), overlying a brown silt loam C horizon (117-152 cmbs). Verdigris series soils have potential for buried horizons below the C horizon (60 inches/152 cmbs), but these pedons are not considered diagnostic. Verdigris series soils are mapped bisecting the study area along Bird Creek near the middle of the study area.

Other than the Verdigris series, no other potential for buried horizons are mapped within the study area.

Vegetation:

The study area is mapped across Bottomland Forest and Post Oak-Blackjack Forest vegetation. Bottomland Forests are found along major rivers throughout Oklahoma; common species include hackberry, red elm, sugarberry, and green ash. Post Oak-Blackjack Forests are found throughout much of central Oklahoma; important species include a variety of oak trees, black hickory, buckbrush, gum bumelia, Mexican plum, redbud, roughleaf dogwood, and smooth and winged sumac.

According to the USGS Land Cover Map, the study area is comprised of developed land within the existing SH-66 right-of-way, and hay pastures, deciduous forests, and herbaceous prairie lands in portions of the study area beyond existing right-of-way. Google Earth imagery is consistent with the Land Cover Map and illustrates sodded right-of-way, pastures, forested areas, and riparian vegetation along the river and creek. Based on the available imagery, vegetation cover is likely to near 100% throughout the study area.

Surface Visibility:

<u>XXX</u>	0-25%	Sodded right-of-way; wooded areas
_____	25-50%	
_____	50-75%	
<u>XXX</u>	75-100%	Eroded areas, exposed bedrock

3. CULTURAL BACKGROUND:

Background Research:

XXX State Site Files at Oklahoma Archeological Survey (OAS)

State Historic Preservation Office (SHPO) National Register of Historic Places (NRHP) listings, Determinations of Eligibility (DOE) in Oklahoma listings, and Oklahoma

XXX Landmarks Inventory (OLI) files, accessed online

A review of the Oklahoma Archeological Survey (OAS) maps indicate that no previously-recorded archaeological sites are located within the project study area, however nine previously-recorded archaeological sites (34RO15, 34RO144, 34RO150-152, 34RO279, 34RO343, 34RO345, 34RO347) are mapped within the project's one-mile vicinity.

Site 34RO15 is mapped approximately 535 feet east of the project study area. The site form, completed by Sherman Lawton in 1963, was based on a report of archaeological materials, specifically Archaic period points, made by an informant identified as Gregory from Muskogee. This location was visited by A.F. Miller in 1976, *A survey and assessment of the cultural resources of the McClellan-Kerr Arkansas River navigation system in Oklahoma*, and by Richard Drass, an archaeologist with the Oklahoma Archeological Survey in 1982. No archaeological materials were observed during either the 1976 or the 1982 investigations.

Site 34RO144 is mapped at the base of a ridge overlooking Spunky Creek, approximately 4,650 feet southwest of the project study area. The site was recorded by Charles Neel in 1980 as a possible mid-18th century to early 19th century toll bridge including a sandstone slab structure and possible sandstone slab bridge supports along Spunky Creek; materials at the site are reported as including aqua, purple, and clear glass fragments, crockery. Boyd's cap, metal fragments, and coal. This site has not been assessed for NRHP eligibility.

Site 34RO150 is mapped on a high ridge south and east of Bird Creek, approximately 4,935 feet west of the project study area. The site was recorded by Richard Drass during a survey of the Bird Creek Basin in 1982 as a small temporary prehistoric camp with two possible hearths and a surface scatter of lithic artifacts including a small dart point, an axe or hoe preform, bifaces, scrapers, flakes, and cobbles. This site has not been assessed for NRHP eligibility.

Site 34RO151 is mapped on a high knoll near Bird Creek, approximately 5,060 feet west of the project study area. The site was recorded by Richard Drass during a survey of the Bird Creek Basin in 1982 as a large prehistoric camp; materials at the site are described as including a scatter of lithic materials including a dart point, a small dart or arrow point preform, bifaces and biface fragments, cores, cobbles, flake tools, scrapers, utilized flakes, a hammerstone, manos, and flakes. This site has not been assessed for NRHP eligibility.

Site 34RO152 is mapped on a terrace just west of a railroad, approximately 4,110 feet west of the project study area. The site was recorded by Richard Drass during a survey of the Bird Creek Basin in 1982 as a non-extant early to mid-20th century farmstead; materials at the site are described as including a scatter of glass, metal, ceramic, and sandstone footing stones that mark the former location of a house and barn. This site has not been assessed for NRHP eligibility.

Site 34RO279 (River Hill Cemetery) is mapped on a bluff overlooking the Verdigris River in a wooded area, approximately 715 feet west of the project study area. The site was recorded by Kent Dickerson, Ken Shingleton, and Kirstin Miller in 1975 as a 20th century cemetery consisting of approximately 17 graves dating from 1882 to 2012. This site was assessed as not eligible for inclusion in the NRHP. The cemetery is in a wooded area and is not well marked or maintained, and this cemetery will be avoided by the proposed project.

Site 34RO343 is mapped on a terrace overlooking the Verdigris River, approximately 2,410 feet northwest

of the project study area. The site was recorded by Chris and Amy Cojeen in 2010 as a mid-20th century farmstead with a house foundation, a cellar, two 12-inch cement circular casings, a possible water well, and two rectangular stem wall foundations; materials at the site are described as including metal 55-gallon drums, a portable outdoor grill, carpet and carpet padding, a large freezer, pull-tab beverage cans, aluminum food cans, plastic bottles, a 1950s-style Chevrolet truck hood, concrete blocks, red bricks, a white enamel wash basin, pressed metal, and various glass fragments. This site was assessed as not eligible for inclusion in the NRHP.

Site 34RO345 is mapped on a level terrace overlooking Bird Creek, approximately 1,810 feet west of the project study area. The site was recorded by Algonquin Consultants, Inc. in 2011 as an unassigned prehistoric habitation with an earth midden; materials at the site are described as including ceramics, bifaces and biface fragments, unifaces, lithic debitage, wood charcoal fragments, fire-cracked rock, hematite, and “ochre.” This site was not assessed for NRHP eligibility.

Site 34RO347 is mapped on a terrace overlooking Bird Creek and the Verdigris River, approximately 2,710 feet northwest of the project study area. The site was recorded by Cojeen Archaeological Services in 2011 as a mid-20th century farmstead with a concrete block outbuilding with railroad ties intersecting the center of the outbuilding; materials at the site are described as including sheet metal, steel cable, concrete fragments, and modern debris. This site was assessed as not eligible for inclusion in the NRHP.

Robert Brooks includes Rogers County in “Region 3” of his Resource Protection, Process Management manuscript (Brooks 1985). “Region 3” consists of the Ozark Plateau and Cherokee Prairie and is made up of the 11 northeastern-most counties in Oklahoma (Brooks 1985:1). In 1985, the majority of the more than 1,500 sites which had been recorded in Region 3 were located in river valleys with a significantly smaller number of sites recorded in the Cherokee Prairie and upland Ozark Plateau (Brooks 1985:9; Wyckoff and Brooks 1983:50). Region 3 includes sites from Paleoindian, Archaic, Woodland, Village Farming, and 19th and 20th century periods. According to Brooks, Archaic sites in Region three are most heavily concentrated in Cherokee County, and Middle Archaic base camps generally occur along major streams (Brooks 1985:25). In contrast, Woodland sites recorded in Region 3 tend to be located in upland and alluvial valley topography and include base camps, temporary camps, burned rock mounds, and rock shelters (Brooks 1985:64). Village Farming sites are generally located on terraces overlooking major drainages (Brooks 1985:66). Because approximately 7% (as of 1985) of the sites in Region 3 had been tested or excavated, we have more substantial data on cultural sequences and assemblage variation for this region than for other regions in Oklahoma (Brooks 1985:9). However, because research has focused on large sites and reservoir projects, data for Region 3 is largely limited to village sites and findings along major streams and rivers, more ancient sites (e.g. Paleoindian) and the prehistoric occupation of uplands and the use of uplands resources in this region are still poorly understood (Brooks 1985:9). According to the Oklahoma Atlas of Archaeological Sites and Management Activities, in 2004, 327 archaeological sites had been recorded in Rogers County (Brooks 2005). Currently there are 366 archaeological sites recorded in Rogers County as a whole.

Prehistoric archaeological sites recorded in the general region of the project, specifically those mapped on the Catoosa topographic quadrangle map, are generally recorded on terraces, rises, and ridges overlooking drainages, especially the Verdigris River, Bird Creek, and Spunky Creek. Previously recorded prehistoric sites in the project’s general area have been identified by surface scatters of lithic artifacts. Nineteenth and 20th century archaeological sites are generally recorded where buildings or occupations are indicated on historic maps and/or aerial photographs. Three non-extant structures are indicated on maps and aeriels dating to the late-19th to mid-20th centuries in or adjacent to the project study area. Each of these locations were field checked as they may represent 19th or 20th century archaeological sites.

Two known cemeteries are located within the project study area’s one-mile vicinity (River Hill Cemetery and Marshall Cemetery). The River Hill Cemetery is located approximately 865 feet west of the current study area; this cemetery is not maintained and does not have clear boundaries. The Marshall Cemetery is located approximately 2,735 feet south of the current study area; this cemetery is not maintained and does not have clear boundaries. Both cemetery locations will be avoided by the proposed project.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge,

containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688)]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C (Spans of Time Reevaluation 2007: 2) but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

The segment of Route 66 within the project was originally constructed as a 30-foot wide roadway in 1925. The original roadway followed the alignment of the existing westbound lanes. In 1927 the roadway was upgraded to include 18-foot wide paving with Portland Concrete. In 1936, the roadway was converted to a four-lane divided facility with a 44-foot wide total paved area. In 1958, the facility was widened to accommodate a 48-foot wide total paved area. In 1970, a full depth reconstruction of the road was completed with Portland Concrete over hot sand asphalt matching the prior 48-foot wide total paved area. Asphalt overlays were applied to the road segment in 1988, 2004, and 2018. The segment of Route 66 within the project is not one of the NRHP-eligible segments designated in the 2002 Oklahoma Route 66 Historic Resources Survey, 1926-1970. According to the 2002 report, historic route 66 followed the westbound lane and not the eastbound lanes; the historic route digressed from the westbound lanes of the four-lane near the navigation system. In 1936, this alignment was bypassed from that point westward to a connection with the 1926 route just beyond the 1936 river bridge. In 1957, the highway in this area was four-laned adding two or four lanes where needed. The approaches to the navigation system were later elevated when its buildings were built in the 1960s.

4. METHODOLOGY:

The methodology for archaeological survey was confined to the eastern half of the existing SH-66 right-of-way throughout the project study area (25.29 acres; 150 feet from the center of the SH-66 median, see Figure 2). This methodology comprised pedestrian archaeological survey with shovel tests excavated at 30-meter intervals and screened through ¼" hardware cloth. Locations where buildings and/or occupations were indicated by historic maps and/or aerial photographs were identified in the field, inspected, and shovel tested for evidence of archaeological materials.

5. RESULTS OF INVESTIGATION:

XXX No archeological sites or buildings recorded in study area.

XXX Resources recorded in study area assessed as **not eligible** for the NRHP. Forms being submitted for agency review.

_____ Oklahoma Archeological Site Survey Form(s) for State Archeologist files.

XXX Historic Preservation Resource Identification Form(s) for SHPO files.

_____ Oklahoma Bridge Survey and Inventory Form.

XXX **NRHP-eligible properties** recorded in study area.

Forms being submitted for agency review.

_____ Oklahoma Archeological Site Survey Form(s) for State Archeologist files.

_____ Historic Preservation Resource Identification Form(s) for SHPO files.

_____ Oklahoma Bridge Survey and Inventory Form.

_____ Archeological sites requiring further assessment (i.e. evaluative testing)

COMMENTS AND DESCRIPTION OF FINDINGS:

No archaeological sites or buildings were recorded or documented in the current project study area.

Site 34RO15 is mapped in the project vicinity. This site was initially mapped based on the reported location where a collector had recovered Archaic period points eroding from the riverbank. Two prior cultural resource investigations (Miller 1976 and Drass 1982) investigated this location and identified no archaeological materials. At the time of the current investigation, the southeast riverbank of Bird Creek within and adjacent to the project study area was walked and the surface and the cut bank were inspected. All shovel tests excavated in the vicinity of this site were negative; no archaeological materials or buried soil horizons were observed and there is no indication that this site extended into the current study area.

Shovel tests excavated in the northeastern portion of the study area were comprised of dark brown silty clay loam (approximately 0-20 cmbs) which overlay dark yellow-brown silty clay loam. Immediately north of the Bird Creek shovel and auger tests comprised dark brown sandy clay loam (0-40 cmbs), which overlay mottled brown loam and tan sand (approximately 40-80 cmbs), which overlay medium brown/tan loamy sand (approximately 80-100 cmbs), which overlay brown loamy clay (approximately 100-120 cmbs), which overlay brown clayey sand (approximately 120-160). Immediately south of Bird Creek shovel and auger tests comprised brown/light brown loamy sand (approximately 0-50 cmbs), which overlay mottled brown loamy sand with bits of brown loamy clay (approximately 50-70 cmbs), which overlay dark brown loamy clay (approximately 70-100 cmbs), which overlay mottled dark brown and reddish brown loamy clay. No soils or sediments consistent with the Verdigris series were observed in shovel or auger tests excavated in the project study area.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

The segment of Route 66 within the project was originally constructed as a 30-foot wide roadway in 1925. The original roadway followed the alignment of the existing westbound lanes. In 1927 the roadway was upgraded to include 18-foot wide paving with Portland Concrete. In 1936, the roadway was converted to a four-lane divided facility with a 44-foot wide total paved area. In 1958, the facility was widened to accommodate a 48-foot wide total paved area. In 1970, a full depth reconstruction of the road was completed with Portland Concrete over hot sand asphalt matching the prior 48-foot wide total paved area. Asphalt overlays were applied to the road segment in 1988, 2004, and 2018.

The segment of Route 66 within the project is not one of the NRHP-eligible segments designated in the 2002 Oklahoma Route 66 Historic Resources Survey, 1926-1970. According to the 2002 report, historic route 66 followed the westbound lane and not the eastbound lanes; the historic route digressed from the westbound lanes of the four-lane near the navigation system. In 1936, this alignment was bypassed from that point westward to a connection with the 1926 route just beyond the 1936 river bridge. In 1957, the highway in this area was four-laned adding two or four lanes where needed. The approaches to the navigation system were later elevated when its buildings were built in the 1960s.

It is our opinion that this segment of roadway retains its association with Route 66, however pursuant to 36

CFR 60.4, it does not retain integrity of design, materials, workmanship, and setting, and that these aspects of integrity are critical for the roadbed to merit consideration for inclusion in the NRHP. Please find attached the Historic Preservation Resource Identification form for this segment of Route 66.

Pursuant to 36 CFR 60.4, it is our opinion this segment of Route 66 lacks historic integrity and is not eligible for inclusion in the NRHP.

6. RECOMMENDATIONS:

XXX **Plan Notes** requiring avoidance of cultural resources in off-project areas

 Approval Recommended with the proposed project as planned with no additional research. If subsurface archaeological materials are exposed during construction, the Contractor and Resident Engineer shall notify the Department Archaeologist in accordance with Section 202.04(a), Standard Specifications for Highway Construction.

XXX **Approval NOT Recommended**, until one or more of the following measures are completed.

XXX **Additional consultation with SHPO** regarding NRHP-eligible Properties

 Revise design to avoid/protect resources

 NRHP Eligibility Archaeological Test Excavations

 Implementation of MOA with SHPO regarding Mitigation of Adverse Effects to Historic Properties

SUMMARY AND COMMENTS REGARDING RECOMMENDATIONS:

No archaeological deposits were observed or recorded during the current investigation; there are no buildings in the project study area.

The existing (eastern) bridge carrying SH-66 northbound over Bird Creek, is a steel truss-thru bridge, containing three K-thru truss spans flanked by two pony trusses on the east (north) and one pony truss on the west (south); the bridge was constructed in 1957 [ODOT Structure # 6602 0368 EX / NBI 13688)]. This bridge, along with its former twin, which was replaced in 2012 (SHPO File #0589-08), was assessed during the Oklahoma Historic Bridge Survey revision (2007) and determined eligible for inclusion in the NRHP under Criterion A for its association with Route 66 and under Criterion C for its engineering characteristics. The 2007 Spans of Time reevaluation identified and evaluated bridges under criteria A and C (Spans of Time Reevaluation 2007: 2) but did not specifically identify the character defining features or aspects of integrity for each structure. This bridge is an historic property and will require additional consultation with the State Historic Preservation Office (SHPO).

The segment of Route 66 within the project was originally constructed as a 30-foot wide roadway in 1925. The original roadway followed the alignment of the existing westbound lanes and has undergone considerable change including a full depth reconstruction in 1970. It is our opinion the segment of Route 66 within the current ODOT project study area retains its association with Route 66, however it does not retain integrity of design, materials, workmanship, or setting. It is our opinion this segment of Route 66 lacks historic integrity and is not eligible for inclusion in the NRHP.

In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the project study area and has identified no additional historic properties. Additional consultation with SHPO and other consulting parties will be necessary to avoid, minimize, mitigate effects to the existing NRHP eligible SH-66 northbound (eastern) bridge over Bird Creek.

Avoidance Areas:

In order to avoid impacts to cultural resources that have not been assessed for NRHP eligibility in the project vicinity by off-project activity such as borrow pit excavation or staging of heavy equipment, it is recommended that the following areas be avoided for the establishment of off-project facilities:

T20N R15E

Section 16: SW $\frac{1}{4}$ SW $\frac{1}{4}$

Section 16: SE $\frac{1}{4}$ SW $\frac{1}{4}$

Section 17: NE $\frac{1}{4}$ SW $\frac{1}{4}$

Section 17: SW $\frac{1}{4}$ SW $\frac{1}{4}$

Section 17: SE $\frac{1}{4}$ NE $\frac{1}{4}$

Section 17: SE $\frac{1}{4}$

Section 20: SW $\frac{1}{4}$ SW $\frac{1}{4}$

REFERENCES

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 USGS Catoosa 7.5' Quadrangle [1963 (1980)]
 USGS Claremore 30' Quadrangle (1901, 1914, 1916)
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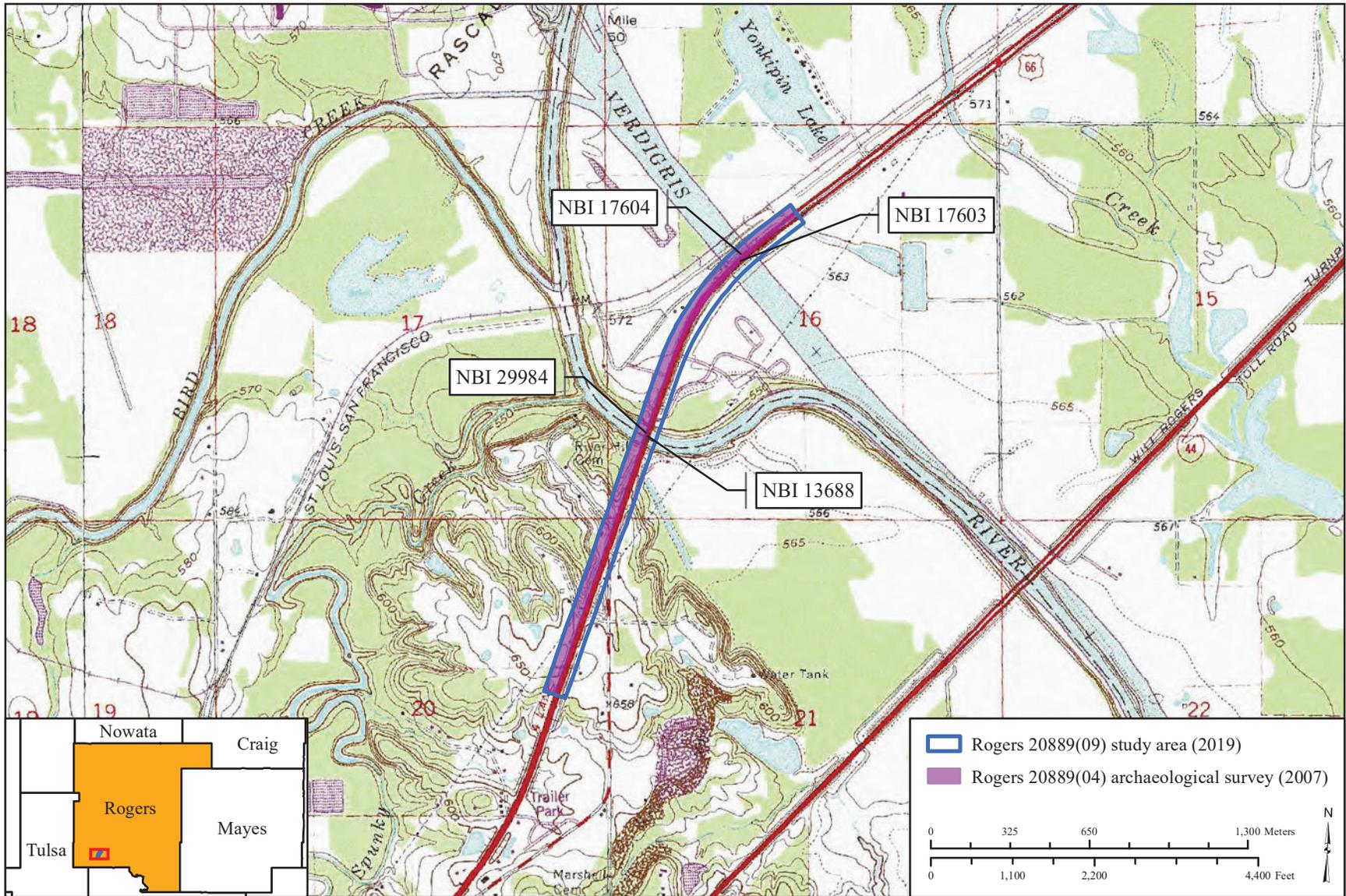


Figure 1. Rogers County JP 20899(09): SH-66 over Bird Creek (northbound/eastbound).

Basemap: USGS Catoosa 7.5' Quadrangle [1963 (1980)]
 Legal location: T20N R15E Sections 16, 20-21



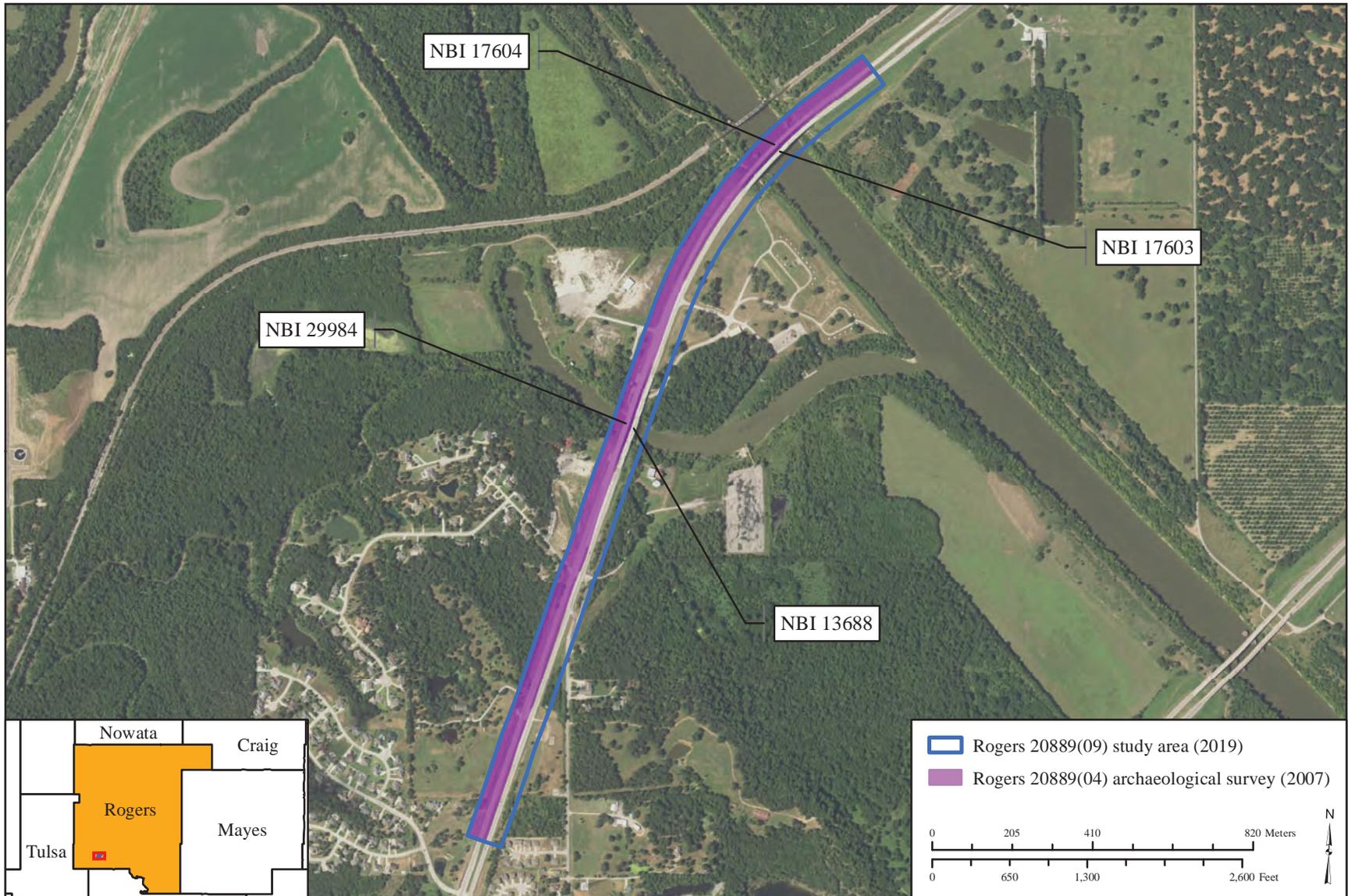


Figure 2. Rogers County JP 20899(09): SH-66 over Bird Creek (northbound/eastbound).

Basemap: USGS Aerial Imagery (2015)
 Legal location: T20N R15E Sections 16, 20-21





Figure 3. Rogers 20899(09): SH-66 over Bird Creek (northbound), study area photos: (a) sodded right-of-way at northeastern end study area, facing southwest; (b) wooded area in right-of-way near middle of study area, facing southwest.



Lorem Ipsum

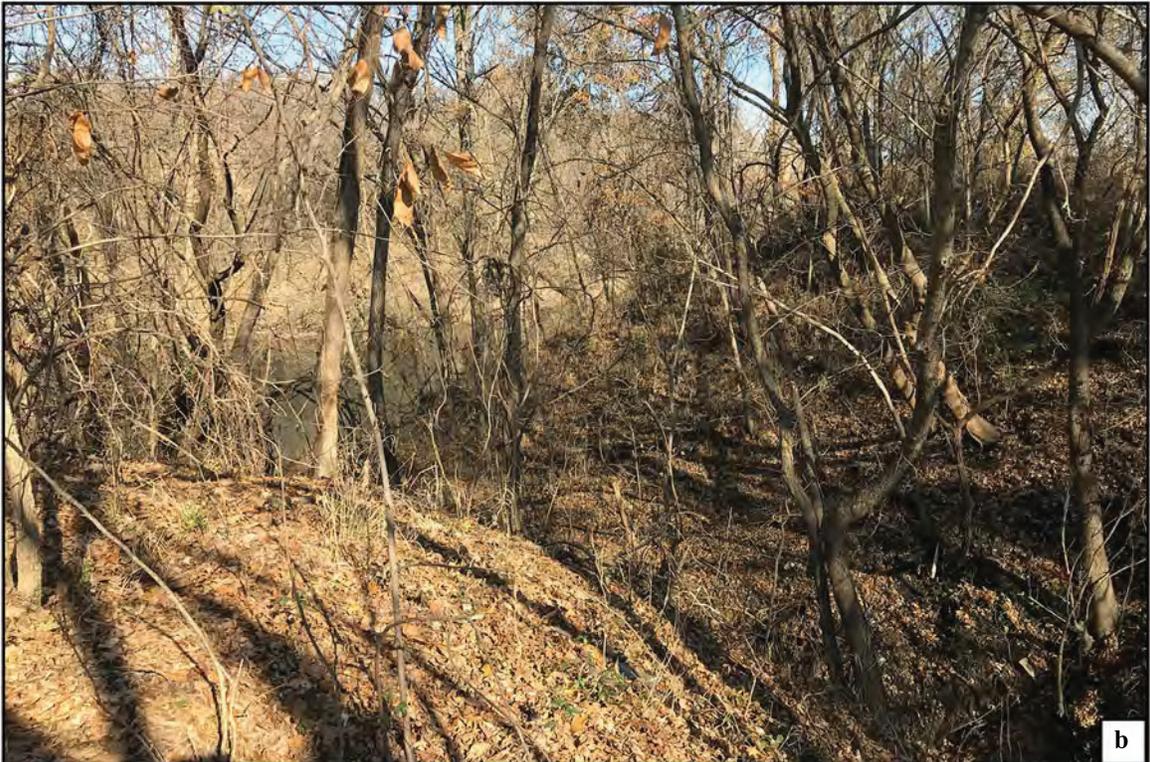


Figure 4a. Rogers 20899(09): SH-66 over Bird Creek (northbound), Bird Creek photos: (a) across Bird Creek, facing southwest; (b) across Bird Creek along a drainage, facing northeast.





Figure 4b. Rogers 20899(09): SH-66 over Bird Creek (northbound), Verdigris River photos: (a) across the Verdigris River, facing southwest; (b) across the Verdigris River, facing northeast.





Oklahoma Historical Society

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State Historic Preservation Office

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December 11, 2019

Mr. Scott Sundermeyer, Director
ODOT Cultural Resources Program
111 East Chesapeake, Rm. 102, OU
Norman, OK 73019

RE: File #0562-20; Bird Creek Bridge on SH-66 Proposed Improvements Project #JP20899(09),
Rogers County

Dear Mr. Sundermeyer:

On November 25, 2019, we received for review the materials for the referenced undertaking submitted with your letter dated November 22, 2019. We concur with the defined area of potential effect (APE) with respect to direct impacts for this project and consider it appropriate for the scope of work.

However, due to the fact that the westbound bridge, a twin to this bridge, was demolished in 2012, this proposed project for the eastbound bridge may be considered a cumulative adverse effect. In addition, as project plans move forward, we recommend that you take into consideration the larger, indirect impact of the project on the archaeological site (34RO15) and the surrounding bridges identified on the map and not just the referenced bridge itself.

Finally, as you consider alternatives, please be sure to describe the previous work conducted on the bridge, including the work described in SHPO File #0852-15 (ODOT Project #JP-20926(05)), and why the repairs have not been successful.

Thank you for the opportunity to review and comment on this project. If you have any questions, please call Catharine M. Wood, Historical Archaeologist, at 405/521-6381. Please reference the above underlined file number when responding. Thank you.

Sincerely,

Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm



**OKLAHOMA DEPARTMENT OF TRANSPORTATION
CULTURAL RESOURCES PROGRAM**

111 E. Chesapeake, Room 102, University of Oklahoma
Norman, OK 73019-5111
Phone: 405-325-7201/325-8665; FAX: 405-325-7604

November 22, 2019

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
Oklahoma History Center
800 Nazih Zuhdi Dr.
Oklahoma City, Oklahoma 73105-7917

Dear Ms. Ozan:

Re: Rogers County Federal Highway Administration (FHWA) Project: JP 20899(09); Proposed improvements to the historic eastbound bridge carrying SH-66 over Bird Creek.

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), is proposing to address deficiencies on the historic eastbound SH-66 bridge over Bird Creek (ODOT Structure no. 6602 0368 EX/NBI 13688). ODOT is currently undergoing a study to evaluate the structural integrity of the bridge, and review a variety of alternatives that may address the deficiencies in the structure.

The bridge, which has been determined eligible for inclusion on the National Register of Historic Places (NRHP), is a mixed truss, containing three K-thru truss spans that are flanked by two pony trusses on the east and one pony truss on the west. The structure and its former twin, which was replaced in 2012 (SHPO File no. 0589-08) were identified by the 2002 Route 66 Roadbed Study as best representing the upgrade of Route 66 to a modern 4-lane highway.

One previously-recorded prehistoric archaeological site (34RO15) is mapped adjacent to the project study area. The site was recorded by Sherman Lawton in 1963, and the site form notes Archaic period points as materials reportedly recovered from the site. Minimal information is available in the site form, and the site location is imprecise. No evidence of the site was identified by two later surveys of the area (AF Miller in 1976 and Richard Drass in 1982), and the site has not been assessed for NRHP eligibility.

On behalf of FHWA we have initiated consultation with the Caddo Nation, the Cherokee Nation, the Delaware Tribe, Kialegee Tribal Town, the Muscogee (Creek) Nation, the Osage Nation, Thlopthlocco Tribal Town, the United Keetoowah Band of Cherokee, and the Wichita and Affiliated Tribes.

Additionally, we have invited representatives of the City of Catoosa, the Catoosa Historic Society Museum, the Historic Bridge Foundation, the Oklahoma Bridge and Highway Group, the Oklahoma Route 66 Association, Inc, the Oklahoma Tourism and Recreation Department, the Oklahoma Public Archaeology Network, Preservation Oklahoma, the National Park Service Route 66 Corridor Preservation Program, and the Route 66 Road Ahead Partnership to be consulting parties in the Section 106 process (November 11, 2019).

The project study area is a 7,387-foot corridor centered on the bridge and extends 150 feet east from the centerline and will be used as the preliminary APE.

In accordance with 36 CFR 800.4(a), we request any preliminary comments you have to the proposed undertaking.

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AN EQUAL OPPORTUNITY EMPLOYER

If you have any questions regarding this project, please contact me at 405-325-7201 or via email at ssundermeyer@odot.org

Sincerely,

A handwritten signature in black ink, consisting of several loops and a long horizontal stroke extending to the right.

Scott Sundermeyer
Director, ODOT Cultural Resources Program



December 4, 2019

CATOOSA CITY HALL

214 S. Cherokee
P.O. Box 190
Catoosa, OK 74015

P: 918-266-2505
F: 918-266-1687
www.cityofcatoosa.org

Brenda Conley
Mayor/Ward 1

Russ White
Vice-Mayor/Ward 6

Thomas Sweet
Ward 2

Greg Williams
Ward 3

Fred Williams
Ward 4

Kyle Gibson
Ward 5

Jonnie Mott
At-Large

ODOT Cultural Resources
Oklahoma Archeological Survey
111 E. Chesapeake
Norman, OK 73019-5111

RE: Rogers County Federal Highway Administration Project: JP 20899(09); Proposed replacement of the historic eastbound bridge carrying SH-66 over Bird Creek

Dear Mr. Sundermeyer,

The City of Catoosa is requesting to be on the consulting committee for the above-mentioned project. We feel we will be able to provide invaluable information and resources with the organizations involved.

Please feel free to call me with any questions you may have.

Have a great day!

Best Regards,



John Blish
City Manager

November 11, 2019

Dear Stakeholder:

Re: Rogers County Federal Highway Administration (FHWA) Project: JP 20899(09); Proposed replacement of the historic eastbound bridge carrying SH-66 over Bird Creek.

Please consider this letter an invitation to be a consulting party for a above referenced project. Section 106 of the National Historic Preservation Act of 1966 requires all federal agencies to take into account the effects of their projects on properties eligible for, or listed on, the National Register of Historic Places (NRHP). Federal agencies, and their delegates, are required to seek comments from parties who have a demonstrated interest in the historic properties that may be affected by a project, including the State Archaeologist, the Oklahoma State Historic Preservation Officer (SHPO), Native American Tribes, and other organizations. On behalf of FHWA, the Oklahoma Department of Transportation (ODOT) has identified your organization as a party with potential interest in the undertaking and we invite your participation. Please find the project description below.

ODOT, in cooperation with FHWA, is considering improvements to the historic eastbound SH-66 bridge over Bird Creek (ODOT Structure no. 6602 0368 EX/NBI 13688). ODOT is currently undergoing a study to evaluate the structural integrity of the bridge and review a variety of alternatives that may address the deficiencies in the structure. The exact project scope and requirements will be clarified through the planning, environmental review, and design process; however, the improvements are considered necessary.

The bridge is mixed truss, containing three K-thru truss spans that are flanked by two pony trusses on the east and one pony truss on the west. The most recent inspection report lists the sufficiency rating at 49.40 out of 100. According to data obtained in 2016, the structure carries 6,750 vehicles per day. The structure, along with its former twin, which was replaced in 2012, was identified by the 2002 Route 66 Roadbed Study as best representing the upgrade of Route 66 to a modern 4-lane highway.

ODOT recognizes that you may have knowledge of other potential cultural or historical resources, or comments regarding the known resource that ODOT and FHWA should consider in planning the project. We formally invite your organization to participate as a Section 106 consulting party for this project and provide any comments or information regarding cultural resources that may be affected.

We respectfully request for a response within 30 days of receipt of this letter. If you indicate an interest in being a consulting party, your organization will be included in receiving information regarding ODOT's cultural resource studies within the project area and the recommendations and determinations resulting from those studies. If you do not respond or respond indicating that you do not desire to be a consulting party, you will not be included on the list of consulting parties and will receive no additional correspondence.

If you have any questions regarding this project, please contact Scott Sundermeyer ODOT Cultural Resources Program, Director at 405-325-7201 or via email at ssundermeyer@odot.org.



OKLAHOMA DEPARTMENT OF TRANSPORTATION
CULTURAL RESOURCES PROGRAM

111 E. Chesapeake, Room 102
Norman 73019-5111
www.odotculturalresources.info

Sincerely,

A handwritten signature in black ink, appearing to read "Jenny Droscher", is written in a cursive style.

Jenny Droscher
Public Involvement Officer, ODOT Cultural Resources Program

Cc: Catoosa Historic Society Museum
City of Catoosa
Historic Bridge Foundation
National Park Service Route 66 Corridor Preservation Program
Oklahoma Bridge and Highway Group
Oklahoma Route 66 Association, Inc.
Oklahoma Tourism and Recreation Department
Oklahoma Public Archeology Network
Preservation Oklahoma
Route 66 Road Ahead Partnership

The Oklahoma Department of Transportation (ODOT) ensures that no person or groups of persons shall, on the grounds of race, color, sex, religion, national origin, age, disability, retaliation or genetic information, be excluded from participation in, be denied the benefits of, or be otherwise subjected to discrimination under any and all programs, services, or activities administered by ODOT, its recipients, sub-recipients, and contractors. To request an accommodation please contact the ADA Coordinator at 405-521-4140 or the Oklahoma Relay Service at 1-800-722-0353. If you have any ADA or Title VI questions email ODOT-ada-titlevi@odot.org.

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AN EQUAL OPPORTUNITY EMPLOYER

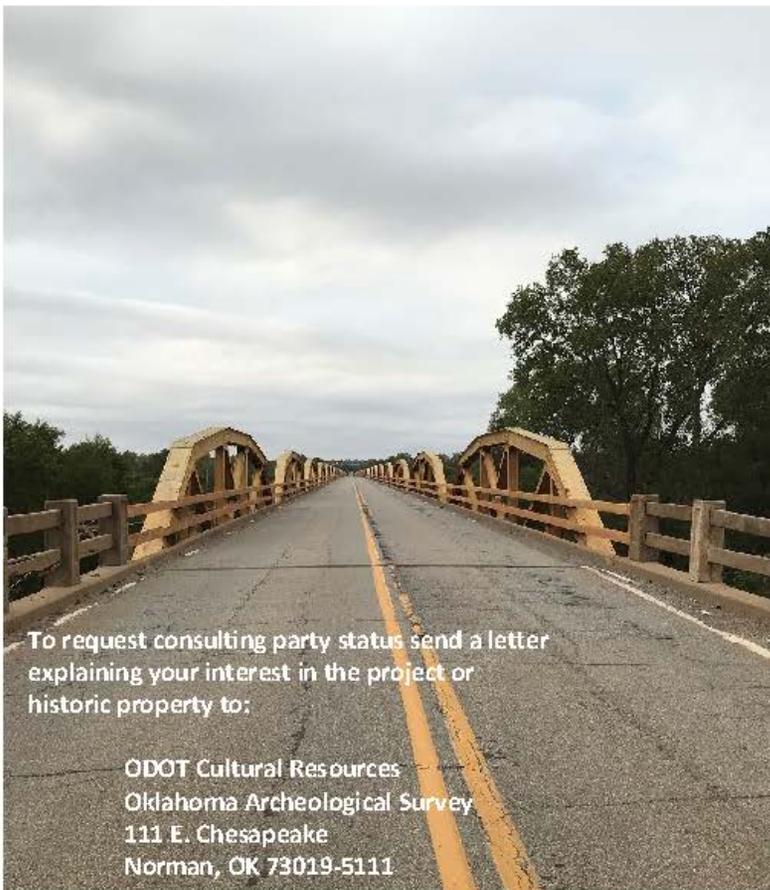


Section 106 Public Involvement



Members of the public can be involved in the Section 106 process by identifying and evaluating resources that are historically important. You, as a member of the public, can participate in the process by expressing your concerns to the Oklahoma Department of Transportation (ODOT). It is important for you to let us know what is important to you and why it is important. Input can be provided at public meetings, by contacting representatives of ODOT, or by submitting public comments at the meeting or online at <http://www.odotculturalresources.info>.

The Section 106 review process gives you the opportunity to alert the federal agency to the historic properties you value, and influence decisions about projects that affect them. If you have a special interest in a property and would like to play a more formal role in the Section 106 process, you may want to become a consulting party.



To request consulting party status send a letter explaining your interest in the project or historic property to:

ODOT Cultural Resources
Oklahoma Archeological Survey
111 E. Chesapeake
Norman, OK 73019-5111

Consulting parties can include:

- Federal agencies
- State Historic Preservation Office
- Representatives of local governments with jurisdiction over the area with historic properties
- Applicants for federal funds
- Additional consulting parties include those with a *demonstrated interest* in the undertaking due to their legal or economic interest in the project or property OR the project effects on historic properties.

Consultation is the process of seeking, discussing and considering the views of others, and where feasible, seeking agreement with them on how historic properties should be identified, considered and managed. Consultation is built on the exchange of ideas.

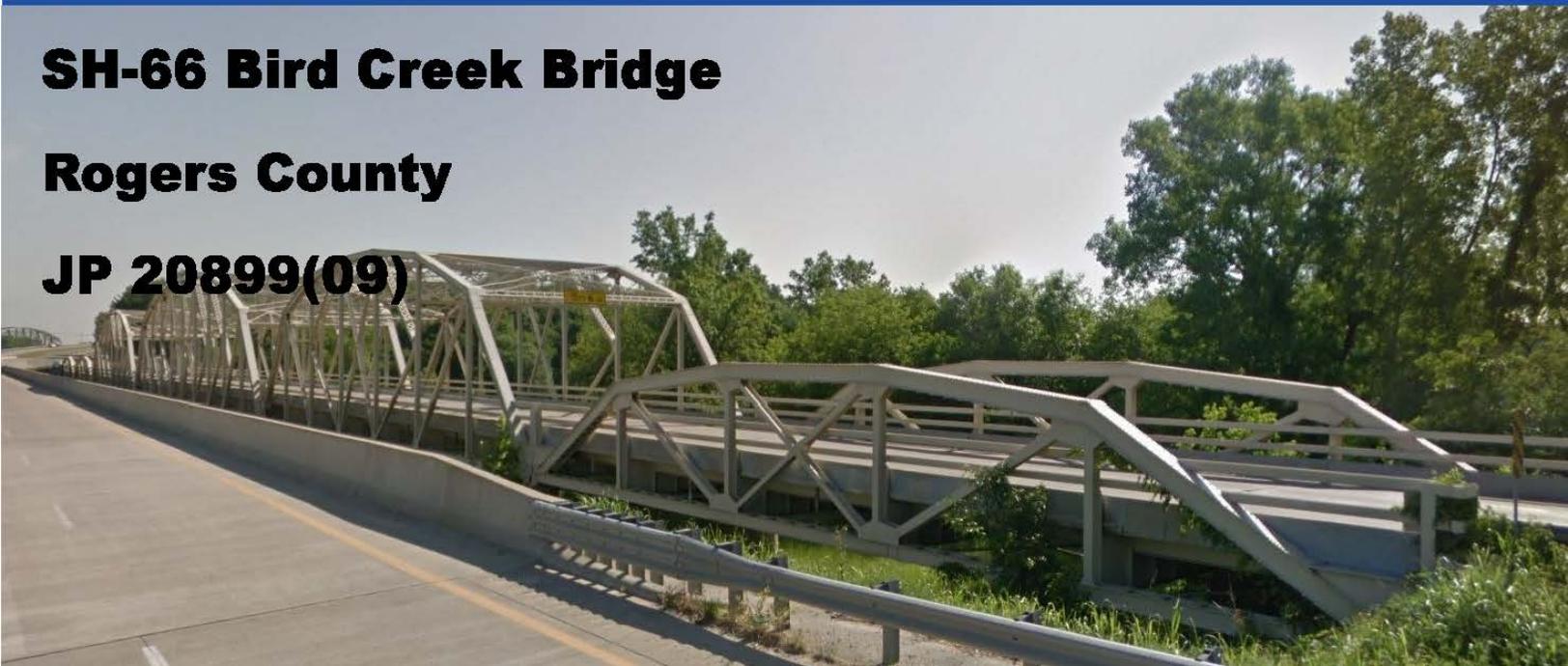
Consultation occurs at key stages of each project including:

- The identification of properties eligible for listing on the National Register of Historic Places,
- The assessment of effects on those properties, and
- Resolution of adverse effects.

SH-66 Bird Creek Bridge

Rogers County

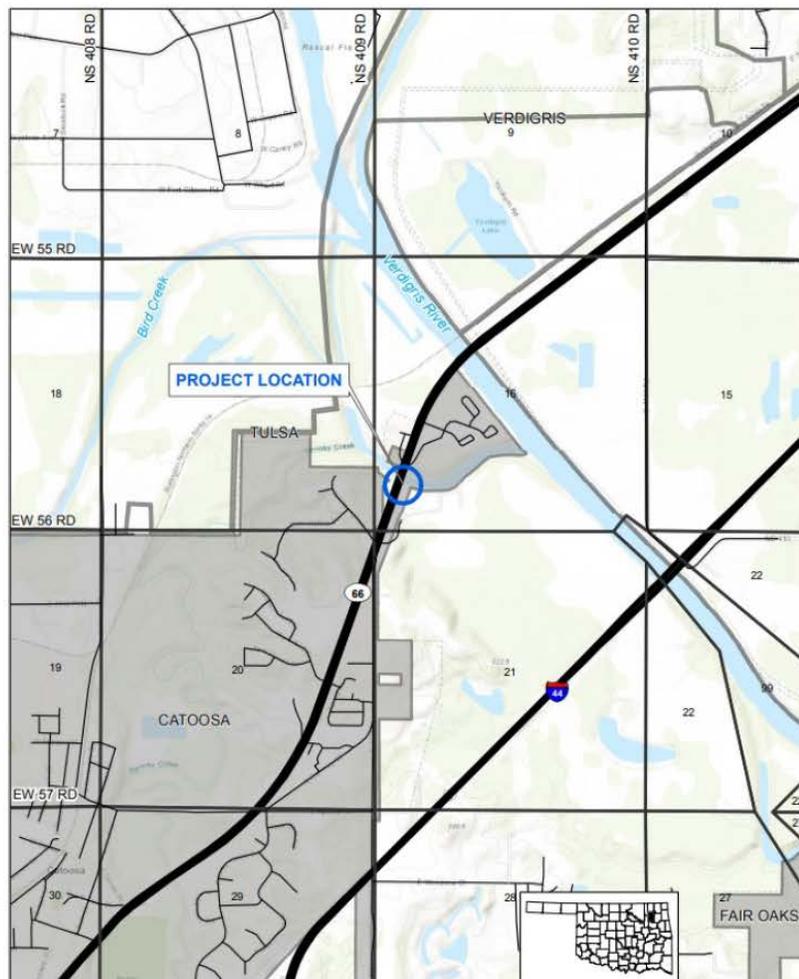
JP 20899(09)



Project Background

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), is considering improvements to the historic eastbound SH-66 bridge over Bird Creek. ODOT is currently undergoing a study to evaluate the structural integrity of the bridge, and review a variety of alternatives that may address the deficiencies in the structure.

The bridge is mixed truss, containing three K-thru truss spans that are flanked by two pony trusses on the east and one pony truss on the west. The most recent inspection report lists the sufficiency rating at 49.40 out of 100. According to data obtained in 2016, the structure carries 6,750 vehicles per day. The structure, along with its former twin, which was replaced in 2012, was identified by the 2002 Route 66 Roadbed Study as best representing the upgrade of Route 66 to a modern 4-lane highway.



Should you have any questions regarding the project, please contact Mr. Scott Sundermeyer, ODOT Cultural Resources Program Director, at (405) 325-7201, ssundermeyer@odot.org.



October 3, 2019

Caddo Nation
 Attn: Chairman Tamara Francis
 P.O. Box 487
 Binger, OK 73009

Dear Chairman Francis:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
Location	Sec 16 T20N R15E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Tribal Historic Preservation Office

May 1, 2020

Caddo Nation
Attn: Chairman Tamara Francis
P.O. Box 487
Binger, OK 73009

Dear Chairman Francis:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

In accordance with 36 CFR Part 800.4, the proposed project area was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation did not identify any archaeological resources in the project area. Our opinion is that this portion of Route 66 does not retain historic integrity and is not eligible for inclusion in the National Register of Historic Places. ODOT will continue consultation with the Oklahoma SHPO regarding the eligible bridge over Bird Creek. In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. Tribal consultation identified sensitive areas located outside of the project's area of potential effect, and these locations have been specified for avoidance by off-site activities.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Tribal Historic Preservation Office

October 3, 2019

Cherokee Nation
 Attn: Principal Chief Chuck Hoskin, Jr.
 P.O. Box 948
 Tahlequah, OK 74465

Dear Principal Chief Hoskin:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
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Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Elizabeth Toombs, THPO

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Office of the Chief

Chuck Hoskin Jr.
Principal Chief

Bryan Warner
Deputy Principal Chief

October 31, 2019

Rhonda Fair
Oklahoma Department of Transportation
Tribal Coordination
200 N.E. 21st Street, Room 3A8
Oklahoma City, OK 73105-3204

Re: JP 20899(09), Bridge replacement and approach improvements on SH-66 over Bird Creek (northbound) and road under, 3.68 miles north of I-44

Dr. Rhonda Fair:

The Cherokee Nation (Nation) is in receipt of your correspondence about **JP 20899(09)**, and appreciates the opportunity to provide comment upon this project. Please allow this letter to serve as the Nation's interest in acting as a consulting party to this proposed project.

The Nation maintains databases and records of cultural, historic, and pre-historic resources in this area. Our Historic Preservation Office reviewed this project, cross referenced the project's legal description against our information, and found no instances where this project is within close proximity to the following resources:

- A culturally sensitive resource located in the SE 1/4 SE 1/4, S17, T20N, R15E; and
- The Crutchfield/Wofford Ferry site located in the SE 1/4 NE 1/4, S17, T20N, R15E.

Thus, the Nation recommends that a cultural resources survey is conducted for this project, and requests a copy of the related report. The Nation requires that cultural resources survey personnel and reports meet the Secretary of Interior's standards and guidelines.

However, the Nation requests that the Oklahoma Department of Transportation (ODOT) halt all project activities immediately and re-contact our Offices for further consultation if items of cultural significance are discovered during the course of this survey.

Additionally, the Nation requests that the Oklahoma Department of Transportation conduct appropriate inquiries with other pertinent Historic Preservation Offices regarding historic and prehistoric resources not included in the Nation's databases or records.

JP 20899(09)
October 31, 2019
Page 2 of 2

If you require additional information or have any questions, please contact me at your convenience.
Thank you for your time and attention to this matter.

Wado,

A handwritten signature in blue ink that reads "Elizabeth Toombs". The signature is written in a cursive style with a large initial "E".

Elizabeth Toombs, Tribal Historic Preservation Officer
Cherokee Nation Tribal Historic Preservation Office
elizabeth-toombs@cherokee.org
918.453.5389

May 1, 2020

Cherokee Nation
Attn: Principal Chief Chuck Hoskin, Jr.
P.O. Box 948
Tahlequah, OK 74465

Dear Principal Chief Hoskin:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

In accordance with 36 CFR Part 800.4, the proposed project area was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation did not identify any archaeological resources in the project area. Our opinion is that this portion of Route 66 does not retain historic integrity and is not eligible for inclusion in the National Register of Historic Places. ODOT will continue consultation with the Oklahoma SHPO regarding the eligible bridge over Bird Creek. In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. Tribal consultation identified sensitive areas located outside of the project's area of potential effect, and these locations have been specified for avoidance by off-site activities.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Elizabeth Toombs, THPO



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Office of the Chief

Chuck Hoskin Jr.
Principal Chief

Bryan Warner
Deputy Principal Chief

May 29, 2020

Rhonda Fair
Oklahoma Department of Transportation
Tribal Coordination
200 N.E. 21st Street, Room 3A8
Oklahoma City, OK 73105-3204

Re: JP 20899(09), Bridge replacement and approach improvements on SH-66 over Bird Creek (northbound) and road under, 3.68 miles north of I-44

Dr. Rhonda Fair:

The Cherokee Nation (Nation) is in receipt of your correspondence about and related report for **JP 20899(09)**, and appreciates the opportunity to provide comment upon this project. Please allow this letter to serve as the Nation's interest in acting as a consulting party to this proposed project.

The Nation maintains databases and records of cultural, historic, and pre-historic resources in this area. Our Historic Preservation Office reviewed this project, cross referenced the project's legal description against our information, and found instances where this project is within close proximity to the following resources:

- A culturally sensitive resource located in the SE 1/4 SE 1/4, S17, T20N, R15E; and
- The Crutchfield/Wofford Ferry site located in the SE 1/4 NE 1/4, S17, T20N, R15E.

These aforementioned resources ("Resources") are located outside the proposed Area of Potential Effects (APE) according to the related reports. Thus, this Office does not object to the project proceeding as long as the following stipulations are observed:

- 1) The Nation concerns with Oklahoma Department of Transportation's (ODOT) work plan to avoid these Resources throughout the project's ground-disturbing activities, including indirect activities such as equipment staging;
- 2) The Nation requests additional consultation if there are any changes to the scope of or activities within the APE;
- 3) The Nation requests that ODOT halt all project activities immediately and re-contact our Offices for further consultation if items of cultural significance are discovered during the course of this project;

JP 20899(09)

May 29, 2020

Page 2 of 2

- 4) The Nation requests that ODOT conduct appropriate inquiries with other pertinent Historic Preservation Offices regarding historic and prehistoric resources not included in the Nation's databases or records.

If you require additional information or have any questions, please contact me at your convenience. Thank you for your time and attention to this matter.

Wado,



Elizabeth Toombs, Tribal Historic Preservation Officer
Cherokee Nation Tribal Historic Preservation Office
elizabeth-toombs@cherokee.org
918.453.5389

October 3, 2019

Delaware Tribe of Indians
 Attn: Chief Chester Brooks
 5100 Tuxedo Blvd.
 Bartlesville, OK 74006-2838

Dear Chief Brooks:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

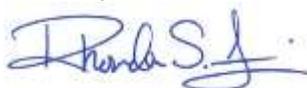
County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
Location	Sec 16 T20N R15E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Brice Obermeyer



Delaware Tribe Historic Preservation Office

1 Kellogg Circle
Roosevelt Hall, RM 212
Emporia State University
Emporia, KS 66801
(620) 341-6699

bobermeyer@delawaretribe.org

October 9, 2019

ODOT
Attn: Rhonda S. Fair
200 N.E. 21st Street, Room 3A8
Oklahoma City, OK 73105-3204

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers
County, Oklahoma; JP# 20899(09)

Dear Rhonda S. Fair,

Thank you for informing the Delaware Tribe on the proposed construction associated with the above referenced project. Our review indicates that there are no religious or culturally significant sites in the project area. As such, we defer comment to your office as well as to the State Historic Preservation Office and/or the State Archaeologist.

We wish to continue as a consulting party on this project and look forward to receiving a copy of the cultural resources survey report if one is performed. We also ask that if any human remains are accidentally unearthed during the course of the survey and/or the construction project that you cease development immediately and inform the Delaware Tribe of Indians of the inadvertent discovery.

If you have any questions, please feel free to contact this office by phone at (620) 341-6699 or by e-mail at bobermeyer@delawaretribe.org

Sincerely,

A handwritten signature in cursive script that reads "Brice Obermeyer".

Brice Obermeyer
Delaware Tribe Historic Preservation Office
1 Kellogg Drive
Roosevelt Hall, RM 212
Emporia State University
Emporia, KS 66801

May 1, 2020

Delaware Tribe of Indians
Attn: Chief Chester Brooks
5100 Tuxedo Blvd.
Bartlesville, OK 74006-2838

Dear Chief Brooks:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

In accordance with 36 CFR Part 800.4, the proposed project area was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation did not identify any archaeological resources in the project area. Our opinion is that this portion of Route 66 does not retain historic integrity and is not eligible for inclusion in the National Register of Historic Places. ODOT will continue consultation with the Oklahoma SHPO regarding the eligible bridge over Bird Creek. In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. Tribal consultation identified sensitive areas located outside of the project's area of potential effect, and these locations have been specified for avoidance by off-site activities.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Brice Obermeyer



Delaware Tribe Historic Preservation Office

1 Kellogg Circle
Roosevelt Hall, RM 212
Emporia State University
Emporia, KS 66801
(620) 341-6699

bobermeyer@delawaretribe.org

May 5, 2020

ODOT
Attn: Rhonda S. Fair
200 N.E. 21st Street
Oklahoma City, OK 73105

Re: Rogers County OK
ODOT JP# 20899(09) cultural resources report

Dear Rhonda S. Fair,

Thank you for providing the survey report for the above referenced project. Our review also indicates that there are no religious or culturally significant sites in this project area and we have no objection to the proposed project. We defer comment to your office as well as to the State Historic Preservation Office and/or the State Archaeologist.

However, we ask that if any human remains are accidentally unearthed during the course of the project that you cease development immediately and inform the Delaware Tribe of Indians of the inadvertent discovery.

If you have any questions, feel free to contact this office by phone at (620) 341-6699 or by e-mail at bobermeyer@delawaretribe.org.

Sincerely,

Brice Obermeyer
Delaware Tribe Historic Preservation Office
1 Kellogg Drive
Roosevelt Hall, RM 212
Emporia State University
Emporia, KS 66801



October 3, 2019

Kialegee Tribal Town
Attn: Mekko Jeremiah Hobia
P.O. Box 332
Wetumka, OK 74883

Dear Mekko Hobia:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

Table with 6 columns: County, Job Piece #, Anticipated Let Date, Project description, Location, Additional information. Contains project details for Rogers County, Job Piece # 20899(09), Anticipated Let Date 2023, Bridge replacement on State Highway 66, etc.

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,

[Handwritten signature of Rhonda S. Fair]

Rhonda S. Fair, Ph.D.
Director
ODOT Tribal Coordination

cc: Historic Preservation Office

May 1, 2020

Kialegee Tribal Town
Attn: Mekko Jeremiah Hobia
P.O. Box 332
Wetumka, OK 74883

Dear Mekko Hobia:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

In accordance with 36 CFR Part 800.4, the proposed project area was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation did not identify any archaeological resources in the project area. Our opinion is that this portion of Route 66 does not retain historic integrity and is not eligible for inclusion in the National Register of Historic Places. ODOT will continue consultation with the Oklahoma SHPO regarding the eligible bridge over Bird Creek. In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. Tribal consultation identified sensitive areas located outside of the project's area of potential effect, and these locations have been specified for avoidance by off-site activities.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Historic Preservation Office

October 3, 2019

Muscogee (Creek) Nation
 Attn: Principal Chief James Floyd
 P.O. Box 580
 Okmulgee, OK 74447

Dear Principal Chief Floyd:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

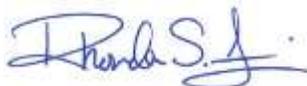
County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
Location	Sec 16 T20N R15E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Tribal Historic Preservation Office

Rhonda Fair

From: Section106 <Section106@mcn-nsn.gov>
Sent: Thursday, October 3, 2019 10:12 AM
To: Rhonda Fair
Subject: RE: Rogers County OK / ODOT JP# 20899(09) initial consultation

Rhonda S. Fair, Ph. D.
Director
ODOT Tribal Coordination
ODT
200 N. E. 21st Street, Room3A8
Oklahoma City, OK 73105-3204

Dr. Fair;

Thank you for contacting the Muscogee (Creek) Nation concerning the Proposed Bridge Replacement and Approach Improvements, JP# 20899(09) on SH – 66 over Bird Creek, Rogers Co., Oklahoma. This project is located outside of our area of interest and we defer to other tribes that have been contacted. Should further information or comment be needed, please do not hesitate to contact me at (918) 732-7852 or by email at djproctor@mcn-nsn.gov.

David J. Proctor

Historic and Cultural Preservation Department, Traditional Cultural Advisor
Muscogee (Creek) Nation
P.O. Box 580 / Okmulgee, OK 74447
T 918.732.7852
F 918.758.0649
djproctor@mcn-nsn.gov
<http://www.muscogeenation-nsn.gov/>

From: Rhonda Fair [mailto:RFair@odot.org]
Sent: Thursday, October 03, 2019 8:43 AM
To: Section106
Subject: Rogers County OK / ODOT JP# 20899(09) initial consultation

Dear David,

Please see the attached project notification. Just let me know if you have any questions.

Thanks!

Rhonda

Rhonda S. Fair, Ph.D.
Director – Tribal Coordination

Oklahoma Department of Transportation



October 3, 2019

Osage Nation
Attn: Principal Chief Geoffrey Standing Bear
627 Grandview
Pawhuska, OK 74056

Dear Principal Chief Standing Bear:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

Table with 6 columns: County, Job Piece #, Anticipated Let Date, Project description, Location, Additional information. Contains details for Rogers County, Job Piece # 20899(09), Anticipated Let Date 2023, and project description regarding bridge replacement on State Highway 66.

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,

Rhonda S. Fair, Ph.D.
Director
ODOT Tribal Coordination

cc: Tribal Historic Preservation Office



Osage Nation Historic Preservation Office

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Date: January 11, 2020

File: 1920-1828OK-10

RE: ODOT, 20899(09), Bridge Replacement and Approach Improvements on SH-66 over Bird Creek, Rogers County, Oklahoma

ODOT
Rhonda Fair
200 NE 21st St Room 1-C1a
Oklahoma City, OK 73105-3204

Dear Dr. Fair,

The Osage Nation Historic Preservation Office has received notification and accompanying information for the proposed project **ODOT, 20899(09), Bridge Replacement and Approach Improvements on SH-66 over Bird Creek, Rogers County, Oklahoma**. There are no known Osage resources within the project area. This office looks forward to reviewing the final report.

Should you have any questions or need any additional information, please feel free to contact me at the number listed below. Thank you for consulting with the Osage Nation on this matter.

Sincerely,

Bobi Deere
Archaeologist

May 1, 2020

Osage Nation
Attn: Principal Chief Geoffrey Standing Bear
627 Grandview
Pawhuska, OK 74056

Dear Principal Chief Standing Bear:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

In accordance with 36 CFR Part 800.4, the proposed project area was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation did not identify any archaeological resources in the project area. Our opinion is that this portion of Route 66 does not retain historic integrity and is not eligible for inclusion in the National Register of Historic Places. ODOT will continue consultation with the Oklahoma SHPO regarding the eligible bridge over Bird Creek. In accordance with 36 CFR 800.4(b), ODOT has completed investigations of the preliminary APE and has identified no additional historic properties. Tribal consultation identified sensitive areas located outside of the project's area of potential effect, and these locations have been specified for avoidance by off-site activities.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Tribal Historic Preservation Office

October 3, 2019

Thlopthlocco Tribal Town
 Attn: Mekko Ryan Morrow
 P.O. Box 188
 Okemah, OK 74859

Dear Mekko Morrow:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

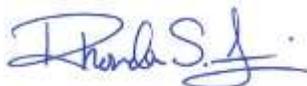
County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
Location	Sec 16 T20N R15E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

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The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Janet Maylen, THPO

May 1, 2020

Thlopthlocco Tribal Town
Attn: Mekko Ryan Morrow
P.O. Box 188
Okemah, OK 74859

Dear Mekko Morrow:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

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If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Galen Cloud

October 3, 2019

United Keetoowah Band of Cherokee
 Attn: Chief Joe Bunch
 P.O. Box 746
 Tahlequah, OK 74465

Dear Chief Bunch:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
Location	Sec 16 T20N R15E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Charlotte Wolf

May 1, 2020

United Keetoowah Band of Cherokee
Attn: Chief Joe Bunch
P.O. Box 746
Tahlequah, OK 74465

Dear Chief Bunch:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

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If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Galen Cloud

October 3, 2019

Wichita & Affiliated Tribes
 Attn: President Terri Parton
 P.O. Box 729
 Anadarko, OK 73005

Dear President Parton:

Re: Section 106 consultation for proposed Federal-Aid undertaking in Rogers County, Oklahoma; JP# 20899(09)

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following project.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				
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Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

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The proposed project area will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the area of potential effect, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.521.3632 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
 Director
 ODOT Tribal Coordination

cc: Mary Botone, THPO

May 1, 2020

Wichita & Affiliated Tribes
Attn: President Terri Parton
P.O. Box 729
Anadarko, OK 73005

Dear President Parton:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Rogers	Job Piece #	20899(09)	Anticipated Let Date	2023
Project description	Bridge replacement and approach improvements on State Highway 66 over Bird Creek (northbound) and road under, 3.68 miles north of Interstate 44				

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If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517-5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Mary Botone, THPO